

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT  
DISTRICT**

**October 3, 2022**

**BOARD OF SUPERVISORS  
REGULAR MEETING  
AGENDA**

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**AGENDA  
LETTER**

# Lake Powell Residential Golf Community Development District

## OFFICE OF THE DISTRICT MANAGER

2300 Glades Road, Suite 410W • Boca Raton, Florida 33431

Phone: (561) 571-0010 • Fax: (561) 571-0013 • Toll-Free: (877) 276-0889

September 26, 2022

**ATTENDEES:**  
Please identify yourself each time you speak to facilitate accurate transcription of meeting minutes.

Board of Supervisors

Lake Powell Residential Golf Community Development District

Dear Board Members:

The Board of Supervisors of the Lake Powell Residential Golf Community Development District will hold a Regular Meeting on October 3, 2022, at 3:00 P.M. (Central Time), at the Boat House, 1110 Prospect Promenade, Panama City Beach, Florida 32413. The agenda is as follows:

1. Call to Order/Roll Call
2. Public Comment
3. Discussion: Bond Refinancing Option(s)
4. District Counsel: Discussion/Consideration/Update
  - A. St. Joe
    - Land Swap – Tennis Courts
    - Cost Share – Covington Bridge Repair Work
  - B. Agreements Sent to POA
    - I. POA Maintenance Agreement
      - Landscape Standards
    - II. POA Agreement for Stormwater Facility Management Services
    - III. POA Boat House Lease
  - C. Other
5. District Engineer: Discussion/Consideration/Update
  - A. 2015 Wild Heron Way Operational Speed Analysis
  - B. Covington Bridge Inspection Report – Bridge #460167
  - C. Drainage Improvements on Intersection of Sweetwater Bay Trail and Wild Heron Way

6. District Ecologist: Discussion/Consideration/Update
  - A. Proposals for Conservation Area Sign Replacements
  - B. Pond Aerators
  - C. Status of Speed Limit Sign Order/Installation (Recommendation from 2015 Speed Analysis)
  - D. Catch Basin Cleanout – CDD Roadway Inspection
  - E. Proposal for Road Side Slope Work
7. Acceptance of Unaudited Financial Statements as of August 31, 2022
8. Approval of August 1, 2022 Public Hearing and Regular Meeting Minutes
9. Staff Reports
  - A. Ecologist/Operations: *Cypress Environmental of Bay County, LLC*
  - B. District Counsel: *Burke Blue*
  - C. District Engineer: *McNeil Carroll Engineering, Inc.*
  - D. District Manager: *Wrathell, Hunt and Associates, LLC*
    - I. Speeding and Traffic Enforcement
      - Machines
      - Steps in Obtaining an MOU
      - Traffic Law Enforcement on Street Signs
      - Mobile Speed Bumps
      - Neighborhood Watch Signs
      - Automated Systems
    - II. NEXT MEETING DATE: December 5, 2022 at 3:00 P.M. (Central Time)
      - QUORUM CHECK

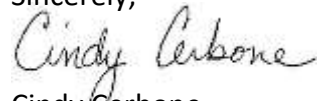
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David Dean	<input type="checkbox"/> IN PERSON	<input type="checkbox"/> PHONE	<input type="checkbox"/> NO
Thomas Balduf	<input type="checkbox"/> IN PERSON	<input type="checkbox"/> PHONE	<input type="checkbox"/> NO
Jerry Robinson	<input type="checkbox"/> IN PERSON	<input type="checkbox"/> PHONE	<input type="checkbox"/> NO
Frank Self	<input type="checkbox"/> IN PERSON	<input type="checkbox"/> PHONE	<input type="checkbox"/> NO

10. Board Member Comments

11. Public Comment
12. Action Item Recap
13. Adjournment

Should you have any questions or concerns, please do not hesitate to contact me directly at (561) 346-5294 or Jamie Sanchez (561) 512-9027.

Sincerely,



Cindy Cerbone  
District Manager

**FOR BOARD MEMBERS AND STAFF TO ATTEND BY TELEPHONE**

**CALL-IN NUMBER: 1-888-354-0094**

**PARTICIPANT PASSCODE: 801-901-3513**

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**5A**

# **WILD HERON WAY OPERATIONAL SPEED ANALYSIS**

**PREPARED FOR:**

**LAKE POWELL RESIDENTIAL GOLF CDD  
IN  
BAY COUNTY, FL**



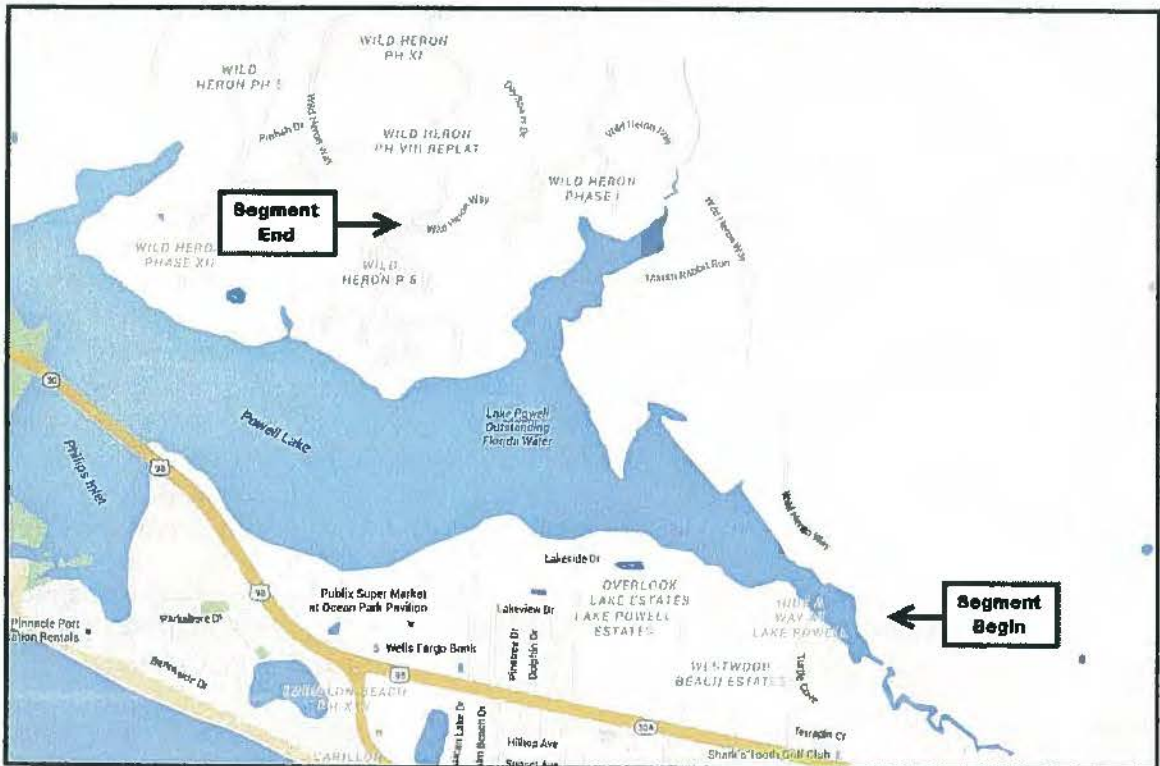
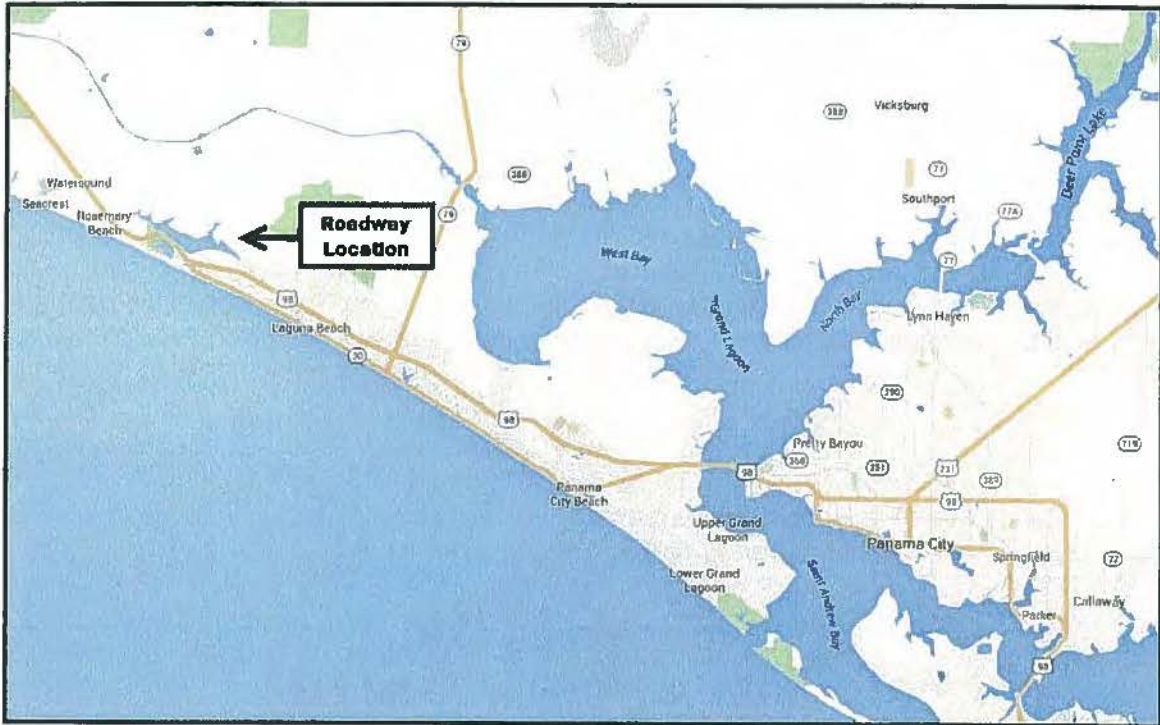
**PREPARED BY:**

**FLORIDA TRANSPORTATION ENGINEERING, INC. (FTE)  
1369 S. RAILROAD AVENUE, SUITE C  
CHIPLEY, FLORIDA 32428**

**PHILLIP KURTH, PE PTOE  
SENIOR TRAFFIC ENGINEER  
FL PE NO. 72434  
12/10/2015**

*Phillip Kurth*  
12/10/15

### Location and Segment Map





## **Introduction**

The Lake Powell Residential Golf Community Development District requested a Speed Zone Study to be conducted on Wild Heron Way in Bay County. *Florida Statutes 316.187 and 316.189* require that an engineering study be conducted before speed limits are altered. The FDOT has developed a manual, *Speed Zoning for Highways, Roads and Streets in Florida, 2010*, which sets forth the procedures of this engineering study. This speed zone study was conducted on Wild Heron Way in accordance with this manual.

## **Roadway Characteristics**

The subject roadway segment is a 2-lane undivided rural residential roadway with 12-foot lane widths. There are no paved shoulders or painted edge lines along the subject roadway. The clear zone on either side of the roadway fluctuates between 15 – 30 feet. The sight distance along this roadway segment is satisfactory. According to data collected on 12/1/2015, the daily traffic volume was 1049 vpd south of the guard station and 951 vpd north / west of the guard station. There is a 90 degree horizontal curve approximately 0.38 mile north of the guard station on Wild Heron Way.

## **Collision Analysis**

If a section of a road has experienced a high frequency of crashes, crash history data should be analyzed for contributing effects of speed. Lowering or raising the posted speed limit may be warranted, depending on predominant crash causes.

Historical crash information from January 1, 2010 to December 31, 2014 was obtained from Signal Four Analytics to determine if vehicular speed was a contributing factor in any of the crashes that might have occurred along the subject roadway segment. For the years 2010 through 2014 there were two (2) reported crashes along Wild Heron Way within the project limits. It was denoted in one (1) of crash reports that excessive vehicular speed was a contributing factor at the time of the collision. The at-fault vehicle was estimated to be traveling at 90 mph approximately 1.0 miles south of Marsh Rabbit Run.

## **Speed Analysis**

According to the *Speed Zoning for Highways, Roads and Streets in Florida, 2010*, the measurement of prevailing speeds of free-flowing traffic during good weather and roadway conditions is the prime requisite for the investigation and the establishments of a speed limit for any roadway segment. Speed limits should normally be set at or near the prevailing or 85th percentile speed of free-flowing, unimpeded traffic. Artificially lowering speed restrictions from that level does not substantially change the actual speeds driven, it creates the perception of a speed trap and generates disrespect for speed zone and other traffic control devices in general. It is commonly accepted in speed zone engineering that roughly 85 percent of drivers maintain prudent and reasonable speed for the variety of highway conditions encountered, regardless of speed limit signs. It is for those drivers who fail to maintain prudent and reasonable speeds that speed zoning exists. Realistic speed restrictions create a more suitable environment for meaningful enforcement to take place.

A Composite Speed Study was conducted along the subject segment of Wild Heron Way on December 7, 2015. For the purposes of this study, the 2.17 mile section of Wild Heron Way was divided into two (2) sub-segments. The sub-segment breakdown is as follows:

- Sub-Segment 1: 0.25 Miles North of SR 30 (US 98) to Guard Station
- Sub-Segment 2: Guard Station to roundabout at Prospect Promenade

The data collection for each sub-segment consisted of a radar Spot Speed Study conducted in accordance with FDOT's *Manual of Uniform Traffic Studies*. The speed data was collected on a typical weekday during good weather conditions and normal roadway operations. The speed data obtained for Sub-Segment 1 was collected between 0.37 miles south of the guard station. The speed data obtained for Sub-Segment 2 was collected 0.72 miles north / west of the guard station. Raw speed data may be found in the study appendix. The results for the radar speed checks are displayed in the following Tables:

TABLE 1		
Wild Heron Way Radar Speed Data		
Sub-Segment 1: 0.37 Miles South of the Guard Station		
Direction	NB	SB
Posted Speed Limit	25	25
<b>85 Percentile Speed</b>	<b>41.7</b>	<b>42.9</b>
10 MPH Pace	31 - 40	36 - 45
Average Speed	36.4	38.4
Highest Recorded Speed	46	47
% Exceeding Posted Speed	100.00	100.00

TABLE 2		
Wild Heron Way Radar Speed Data		
Sub-Segment 2: 0.72 Miles North / West of the Guard Station		
Direction	EB	WB
Posted Speed Limit	25	25
<b>85 Percentile Speed</b>	<b>36.3</b>	<b>34.6</b>
10 MPH Pace	26 - 35	28 - 37
Average Speed	31.9	30.3
Highest Recorded Speed	43	39
% Exceeding Posted Speed	97.67	88.89

According to Section 9 of the *Manual on Speed Zoning for Highways, Roads, and Streets in Florida*, a speed limit should not differ from the 85<sup>th</sup> percentile speed, or the upper limit of the 10 mph Pace Speed by more than 3 mph and it shall not be less than 8 mph. A speed limit of 4 to 8 mph less than the 85<sup>th</sup> percentile speed shall be supported by a supplemental investigation which shows there are road or roadside features not readily obvious to the normally prudent driver, such as length of section, alignment, roadway width, surface condition, sight distance, traffic volume, crash experience, maximum comfortable speed in curves, side street friction, 0.72 miles north / west of the guard station, design speed, etc.

**Supplemental Investigation**

As previously stated, *there are no paved shoulders or painted edge lines along the subject roadway*. In addition, there is a 90 degree horizontal curve approximately 0.38 mile north of the guard station on Wild Heron Way.

Wild Heron Way  
Bay County  
December 10, 2015

### **Recommendations**

Based on the guidelines set forth by the *Manual on Speed Zoning for Highways, Roads, and Streets in Florida*, and the Supplemental Investigation, it is **recommended that the current posted speed of 25 MPH be raised to 35 MPH on Wild Heron Way from 0.25 miles north of SR 30 (US 98) to the Guard Station.** Furthermore, it is **recommended that the current posted speed of 25 MPH be raised to 30 MPH on Wild Heron Way from the Guard Station to 250 feet east of the roundabout at Prospect Promenade.** Finally, it is recommended that a Horizontal Alignment Advisory Speed Sign (MUTCD W1-1a) of 25 MPH should be posted, in both directions, 500 feet in advance of the 90 degree horizontal curve located approximately 0.38 mile north of the guard station on Wild Heron Way.

### **Enforcement**

The enforcement of these speed limits will be the responsibility of the Bay County Sheriff's Office.

Phillip Kurth, PE PTOE  
12/10/2015

# APPENDIX

**WILD HERON WAY – 0.37 MILES SOUTH OF GUARD STATION  
LOOKING NORTH**



**WILD HERON WAY – 0.37 MILES SOUTH OF GUARD STATION LOOKING SOUTH**



**WILD HERON WAY – 0.72 MILES NORTH / WEST OF GUARD STATION  
LOOKING WEST**



**WILD HERON WAY – 0.72 MILES NORTH / WEST OF GUARD STATION  
LOOKING EAST**



DATA COLLECTION SUMMARY SHEET	
LOCATION:	WILD HERON WAY
COUNTY:	BAY
SECTION:	N/A
MP:	0.37 MILES SOUTH OF GAURDHOUSE
STUDY DATE:	12/3/2015
POSTED SPEED	25 MPH

SPEED DATA	NB	SB
85%	43.7	42.9
Average	38.6	38.4
%exceeding	100%	100%
Highest	46	47

FIELD OBSERVATIONS:    Low Volume Roadway





**DATA COLLECTION SUMMARY SHEET**

LOCATION:	WILD HERON WAY
COUNTY:	BAY
SECTION:	N/A
MP:	0.72 MILES N OF GAURDHOUSE
STUDY DATE:	12/3/2015
POSTED SPEED:	25 MPH

**SPEED DATA SUMMARY**

SPEED DATA	EB	WB
85%	36.3	34.6
Average	31.9	30.3
Percentages	97.67%	88.89%
Highest	43	39

FIELD OBSERVATIONS      Low Volume Roadway

Wild Heron Way  
 Bay County  
 December 10, 2015

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION																															
VEHICLE SPOT SPEED STUDY																															
LOCATION ID: WILD HERON WAY N. OF GAURDHOUSE												SECTION: N/A																			
LOCATION: 2.05 MILES N. OF US 98												MP: 0.72 MILES N. OF GAURDHOUSE																			
POSTED SPEED (mph): 25												COUNTY: BAY																			
DATE: 12/7/2015												PAVEMENT CONDITION: DRY																			
OBSERVER: J. GRUBB												TIME FROM: 12:30 PM						TIME TO: 0:30 PM													
REMARKS:																															
NUMBER OF VEHICLES												SPEED												NUMBER OF VEHICLES						BOTH DIRECTIONS	
MILE TOTAL	TOTAL	EB										(mph)	WB										TOTAL	CORR. TOTAL	TOTAL	CORR. TOTAL					
		20	25	30	35	40	45	50	55	60	65		20	25	30	35	40	45	50	55	60	65									
43	0											>65														0	45				
45	0											55															0	45			
48	0											50															0	45			
45	0											53															0	45			
45	0											52															0	45			
45	0											52															0	45			
43	0											50															0	45			
43	0											49															0	45			
43	0											40															0	45			
43	0											47															0	45			
45	0											46															0	45			
43	0											45															0	45			
45	0											46															0	45			
43	1											45															0	45			
42	1											42															0	45			
41	0											42															0	45			
41	1											40															0	45			
40	0											39	1														1	45			
40	2											38	1	1													2	44			
38	3											37	1	1													0	42			
36	0											36	1	1													2	42			
36	4											35	1	1	1												4	40			
33	2											34	1	1													1	36			
30	9											33	1	1	1	1											4	36			
27	1											32	1	1													2	34			
20	1											31	1	1	1	1											0	30			
10	5											30	1	1	1	1	1										6	26			
14	7											28	1	1	1	1											3	20			
12	5											28	1	1	1	1	1	1	1	1	1	1	1	1	1		10	17			
7	1											27	1	1													0	7			
5	5											26	1	1													2	7			
1	0											25	1														1	5			
1	0											24	1	1													2	4			
1	1											23	1	1													0	2			
0	0											22	1	1													2	2			
0	0											21															0	0			
0	0											20															0	0			
0	0											19															0	0			
0	0											18															0	0			
0	0											17															0	0			
0	0											16															0	0			
0	0											15															0	0			
0	0											15															0	0			
0	0											15															0	0			
43	43											45															45	45			
SPEED DATA SUMMARY												EB						WB						BOTH DIRECTIONS		ENGINEER: CHASE STANTON					
85th PERCENTILE SPEED												36.3						34.6								DATE: December 7th, 2015					
10 MPH PACE												26 - 35						28 - 37													

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**5B**



**INSPECTION REPORT SUBMITTAL MEMORANDUM**

TO: Cindy Cerbone  
District Manager  
Wrathell, Hunt and Associates, LLC  
2300 Glades Road # 410W  
Boca Raton, FL 33431

DATE: May 31, 2022  
CONSOR JOB NO.: 18008FL03.00

RE: D3 Local Government Bridge Inspection - Contract No: CA058  
Financial ID Nos: 224859-1-72-11, 224858-1-72-11

**We are pleased to submit the following final inspection reports:**

**TRANSMITTED VIA:**

US Mail     UPS     Hand Delivery     Pick Up     Other: Email

BRIDGE #	Inspection Date	Inspection Type
460167	2/28/2022	Initial

**CONSOR Engineers, LLC**

**Andrea N. Little  
Project Coordinator**

**FLORIDA DEPARTMENT OF TRANSPORTATION**  
**BRIDGE MANAGEMENT SYSTEM**  
**Inspection/CIDR/Bridge Profile Report**  
**Inspection**

Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

BY: Consor Engineers, LLC	STRUCTURE NAME: Not recorded
OWNER: 26 Private(nonRailroad)	YEAR BUILT: 2022
MAINTAINED BY: 26 Private(nonRailroad)	SECTION NO.: 46 000 000
STRUCTURE TYPE: 1 Reinforced Concrete - 22 Channel Beam	MP: 0.300
LOCATION: 0.3 Miles N of SR30A/US98	ROUTE: 00000
SERV. TYPE ON: 1 Highway	FACILITY CARRIED: Wild Heron Way
SERV. TYPE UNDER: 5 Waterway	FEATURE INTERSECTED: Powell Lake

 FUNCTIONALLY OBSOLETE STRUCTURALLY DEFICIENT

TYPE OF INSPECTION: Regular NBI

DATE FIELD INSPECTION WAS PERFORMED: ABOVE WATER: 2/28/2022 UNDERWATER: 2/28/2022

SUFFICIENCY RATING: 79.7  
HEALTH INDEX: 86.97

This report contains information relating to the physical security of a structure and depictions of the structure. This information is confidential and exempt from public inspection pursuant to sections 119.071(3)(a) and 119.071(3)(b), Florida Statutes. Only the cover page of this report may be inspected and copied.

**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM  
Inspection/CIDR/Bridge Profile Report  
Inspection**

Structure ID: 460167

DISTRICT: D3 - Chipley

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STRUCTURE TYPE: 1 Reinforced Concrete - 22 Channel Beam	MP: 0.300
LOCATION: 0.3 Miles N of SR30A/US98	ROUTE: 00000
SERV. TYPE ON: 1 Highway	FACILITY CARRIED: Wild Heron Way
SERV. TYPE UNDER: 5 Waterway	FEATURE INTERSECTED: Powell Lake

- THIS BRIDGE CONTAINS FRACTURE CRITICAL COMPONENTS
- THIS BRIDGE IS SCOUR CRITICAL
- THIS REPORT IDENTIFIES DEFICIENCIES WHICH REQUIRE PROMPT CORRECTIVE ACTION
- FUNCTIONALLY OBSOLETE  STRUCTURALLY DEFICIENT

TYPE OF INSPECTION: Regular NBI  
 DATE FIELD INSPECTION WAS PERFORMED: ABOVE WATER: 2/28/2022 UNDERWATER: 2/28/2022

**OVERALL NBI RATINGS:**

DECK: 7 Good	CHANNEL: 6 Bank Slumping
SUPERSTRUCTURE: 5 Fair	CULVERT: N N/A (NBI)
SUBSTRUCTURE: 5 Fair	SUFF. RATING: 79.7
PERF. RATING: Fair	HEALTH INDEX: 86.97

**FIELD PERSONNEL / TITLE / NUMBER:**

**INITIALS**

Stewart, Joseph - Bridge Inspector (CBI #00631) (lead)	_____
Hithens, Joseph - Assistant Bridge Inspector	_____
Lane, Jeffrey - Bridge Inspector (CBI# 00545) / Lead Diver	<i>JL</i>
Diaz, Daniel - Assistant Bridge Inspector / Diver	_____
Harrison, Andrew - Assistant Bridge Inspector / Diver	_____
Fabian, Marco - Assistant Bridge Inspector / Diver	_____

**REVIEWING BRIDGE INSPECTION SUPERVISOR:**

Akers, Matt - Bridge Inspector (CBI#00386)     *MA*    

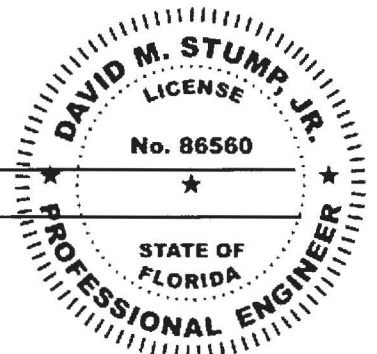
**CONFIRMING REGISTERED PROFESSIONAL ENGINEER:**

Stump, Jr., David M. - Professional Engineer (#86560) CONSOR Engineers, LLC  
 2121 Old Hickory Tree Road  
 Registry No. 6876  
 Saint Cloud FL 34772

This item has been digitally signed and sealed by:  
 David M Stump Jr.  
 2022 04 28

SIGNATURE:     *David M Stump Jr*      
 DATE:     16:34:45-04'00'    

on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.



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**FLORIDA DEPARTMENT OF TRANSPORTATION**  
**BRIDGE MANAGEMENT SYSTEM**  
**Inspection/CIDR/Bridge Profile Report**  
**Inspection**

Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

## All Elements

## DECKS : Decks/Slabs

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	12 / 4	Re Concrete Deck	10197	97.99	209	2.01	0	.	0	.	10406 sq.ft
0	1090 / 4	Exposed Rebar	0	.	1	100	0	.	0	.	1 sq.ft
0	1130 / 4	Cracking (RC and Other)	0	.	208	100	0	.	0	.	208 sq.ft
0	510 / 4	Wearing Surfaces	9365	90	0	.	1041	10	0	.	10406 sq.ft
0	3220 / 4	Crack (Wearing Surface)	0	.	0	.	1041	100	0	.	1041 sq.ft

## Element Inspection Notes:

12/4

CONDITION STATE 2

NEW:

- 1) The underside of Span 1 has exposed steel, up to 0.33 ft long, on the left side, adjacent to Beam 1-1, due to insufficient cover. (1090 - 1 SF)
- 2) The underside of the deck overhang has transverse cracks, 2.0 ft L x 0.02 in W, with efflorescence. (1130 - 208 SF)

NOTES

NEW:

- 1) Approach guardrails are not provided (See Photo 1).
- 2) Object markers are not provided (See Photo 1).
- 3) There is dirt and debris buildup along the shoulders (See Photo 2).
- 4) The roadway striping has faded.
- 5) There are four utility pipes connected to the deck underside attached in all spans to the following beams: Beams 2, 3, 10, and 11 (See Photo 3).

1090/4

-

1130/4

-

510/4

CONDITION STATE 3

NEW:

- 1) The asphalt overlay has longitudinal cracking, full length x 0.13 in W, in the travel lanes (See Photo 4). (3220 - 1041 SF)

3220/4

-

## DECKS : Joints

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	301 / 4	Pourable Joint Seal	301	100	0	.	0	.	0	.	301 ft

## Element Inspection Notes:

301/4

NOTES

NEW:

- 1) Joints are not visible due to asphalt overlay.

## MISCELLANEOUS : Channel

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	8290 / 4	Channel	1	100	0	.	0	.	0	.	1 (EA)

## Element Inspection Notes:

8290/4

No Notes

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**FLORIDA DEPARTMENT OF TRANSPORTATION**  
**BRIDGE MANAGEMENT SYSTEM**  
**Inspection/CIDR/Bridge Profile Report**  
**Inspection**

Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

**MISCELLANEOUS : Other Elements**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	321 / 4	Re Conc Approach Slab	1002	100	0	.	0	.	0	.	1002 sq.ft
0	510 / 4	Wearing Surfaces	1002	100	0	.	0	.	0	.	1002 sq.ft

**Element Inspection Notes:**

321/4

NOTES

NEW:

1) There are transverse cracks, full length x 0.13 in W, at both roadway/approach slab transitions (See Photo 5).

510/4

No Notes

**MISCELLANEOUS : Other Elements**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	8475 / 4	R/Conc Walls	61	95.31	0	.	3	4.69	0	.	64 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	3	100	0	.	3 ft

**Element Inspection Notes:**

8475/4

CONDITION STATE 3

NEW:

1) The near left wingwall has a spall, 1.67 ft H x 0.21 ft W x 0.08 ft D, in the far right corner of the third pile (See Photo 6). (1080 - 1 FT)

2) The near left wingwall has a spall, 1.17 ft H x 0.42 ft W x 0.21 ft D, in the top southwest corner of the fourth pile (See Photo 6). (1080 - 1 FT)

3) The near right wingwall has a spall, 1.0 ft H x 0.29 ft W x 0.13 ft D, in the near left corner of the fourth pile (See Photo 7). (1080 - 1 FT)

1080/4

-

**SUBSTRUCTURE : Substructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	215 / 4	Re Conc Abutment	66	76.74	12	13.95	8	9.3	0	.	86 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	3	100	0	.	3 ft
0	1090 / 4	Exposed Rebar	0	.	0	.	5	100	0	.	5 ft
0	1130 / 4	Cracking (RC and Other)	0	.	12	100	0	.	0	.	12 ft

**Element Inspection Notes:**

215/4

CONDITION STATE 2

NEW:

1) There are diagonal cracks, up to 4.0 ft L x 0.02 in W, in the near backwall. (1130 - 9 FT)

2) Abutment Cap 1 has map cracking, up to 3.0 ft L x 0.02 in W, in the outside face with efflorescence. (1130 - 3 FT)

CONDITION STATE 3

NEW:

1) The near backwall has a spall, 1.5 ft H x 0.17 ft W x 0.25 in D, with exposed steel due to insufficient cover (See Photo 8). (1080 - 1 FT)

2) The near backwall has a spall, 0.58 ft H x 0.17 ft W x 0.25 in D, at ground level between Piles 1-4 and 1-5, with exposed steel, 0.58 ft long, and 50% section loss (See Photo 9). (1090 - 1 FT)

3) The far backwall has a spall, 3.5 ft L x 0.25 ft W x 0.25 ft D, between Piles 7-3 and

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7-4, with one piece of exposed steel and 30% section loss (See Photo 10). (1090 - 4 FT)  
 4) The far backwall has a spall, 0.50 ft H x 1.0 ft L x 0.08 ft D, with exposed steel (See Photo 11). (1080 - 1 FT)  
 5) Abutment Cap 7 has a spall, 0.67 ft L x 0.17 ft H x 0.50 in D, under Beam 4 (See Photo 12). (1080 - 1 FT)

## NOTES

## NEW:

1) The far backwall has cracking/delaminations in the skim coating.

1080/4 -

1090/4 -

1130/4 -

## SUBSTRUCTURE : Substructure

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	227 / 4	Re Conc Pile	0	.	35	71.43	14	28.57	0	.	49 (EA)
0	1080 / 4	Delamination/Spall/Patched Area	0	.	35	85.37	6	14.63	0	.	41 (EA)
0	1130 / 4	Cracking (RC and Other)	0	.	0	.	8	100	0	.	8 (EA)

## Element Inspection Notes:

227/4

CONDITION STATE 2

## NEW:

- 1) All piles have abrasion, 0.04 in deep, in the splash zone. (1190 - NO QTY)
- 2) There are spalls, 0.42 ft diameter x 0.50 in deep, throughout all piles. (1080 - 34 EA)
- 3) Pile 7-3 has a patch, 1.0 ft H X 0.33 ft W, in the near right corner. (1080 - 1 EA)

CONDITION STATE 3

## NEW:

- 1) Pile 2-3 has a spall, 0.50 ft H x 0.21 ft W x 0.50 in D, 5.0 ft below the bent cap (See Photo 13). (1080 - 1 EA)
- 2) Pile 2-7 has a crack, 0.06 in wide, in the far left side (See Photo 14). (1130 - 1 EA)
- 3) Piles 3-2, 3-3, 5-1 and 5-2 have a vertical cracks, 1.0 ft H x 0.06 in W, starting at the channel bottom on the right face (See Photo 14). (1130 - 3 EA)
- 4) Pile 3-4 has cracks, 0.06 in wide, in the left and right face with corrosion bleed out (See Photo 15). (1130 - 1 EA)
- 5) Pile 5-1 has a spall, 0.67 ft H x 0.17 ft W x 0.50 in D, 4.0 ft below the bent cap on the far right corner (See Photo 13). (1080 - 1 EA)
- 6) Pile 5-3 has a vertical crack, 1.0 ft H x 0.06 in W, starting at the channel bottom on the right face, with corrosion bleed out (See Photo 15). (1130 - 1 EA)
- 7) Pile 5-4 has a spall, 0.50 ft H x 0.25 ft W x 0.08 ft D, 5.0 ft below the bent cap on the far right corner (See Photo 13). (1080 - 1 EA)
- 8) Pile 5-6 has a failing patch, 0.33 ft H X 0.42 ft W, at the bent cap on the near right corner (See Photo 16). (1080 - 1 EA)
- 9) Pile 7-1 has a spall, 0.75 ft H x 0.50 ft W x 0.17 ft D, 0.50 ft below the bent cap on the near left corner, with one piece of exposed steel (See Photo 17). (1080 - 1 EA)
- 10) Piles 7-1, 7-4, and 7-5 have a cracks, 1.0 ft H x 0.06 in W, starting at the channel bottom on the near face (See Photo 14). (1130 - 2 EA)
- 11) Pile 7-7 has a failing patch, 1.58 ft H x 0.33 ft W, in the far right corner adjacent to the far backwall (See Photo 16). (1080 - 1 EA)

1080/4 -

1130/4 -

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**SUBSTRUCTURE : Substructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	234 / 4	Re Conc Pier Cap	182	84.65	23	10.7	10	4.65	0	.	215 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	1	100	0	.	0	.	1 ft
0	1130 / 4	Cracking (RC and Other)	0	.	22	68.75	10	31.25	0	.	32 ft

**Element Inspection Notes:**

234/4      CONDITION STATE 2

NEW:

1) There are vertical cracks, up to 2.0 ft H x 0.02 in W, with efflorescence randomly throughout the bent caps. (1130 - 22 FT)

2) Bent Cap 4 has a spall, 0.33 ft H x 0.42 ft L x 0.08 ft D, in the far right face. (1080 - 1 FT)

CONDITION STATE 3

NEW:

1) The left end of Bent Cap 6 has a crack, 0.06 in wide, extending around the near and far faces (See Photo 18). (1130 - 10 FT)

1080/4      -

1130/4      -

**SUPERSTRUCTURE : Bearings**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	310 / 4	Elastomeric Bearing	288	100	0	.	0	.	0	.	288 each

**Element Inspection Notes:**

310/4      No Notes

**SUPERSTRUCTURE : Superstructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	110 / 4	Re Conc Opn Girder/Beam	2767	97.29	0	.	77	2.71	0	.	2844 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	32	100	0	.	32 ft
0	1090 / 4	Exposed Rebar	0	.	0	.	40	100	0	.	40 ft
0	1130 / 4	Cracking (RC and Other)	0	.	0	.	5	100	0	.	5 ft

**Element Inspection Notes:**

110/4      CONDITION STATE 1

NEW:

1) All beams have hairline vertical cracks, up to 1.67 ft H x 0.01 in W, in both faces of each leg, imitating from bolt connection hardware at midspan.

CONDITION STATE 3

NEW:

1) Beam 1-1, left leg has a spall, 0.33 ft L x 0.67 ft W x 0.08 ft D, adjacent to the abutment, with exposed steel, 0.33 ft long (See Photo 19). (1080 - 1 FT)

2) Beam 1-6, right leg has a spall, 1.0 ft H x 0.67 ft L x 0.17 (See Photo 20). (1080 - 1 FT)

3) Beam 1-8, left leg has a spall, 0.92 ft L x 0.67 ft W x 0.08 ft D, at Bent Cap 2 in the bottom face (See Photo 21). (1080 - 1 FT)

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- 4) Beam 2-11, left leg has a spall, 0.83 ft H x 0.17 ft L x 0.17 ft D, 5.0 ft from Bent Cap 3 in the left face, with two pieces of exposed steel, up to 0.17 ft long, with no section loss (See Photo 22). (1080 - 1 FT)
- 5) Beam 4-5, right leg has a spall, 0.83 ft L x 0.58 ft W x 0.50 in D, 7.0 ft from Bent Cap 4 in the bottom face, with two pieces of exposed steel (See Photo 22). (1080 - 1 FT)
- 7) The bottom left face of Beam 4-12 has a spall/delamination, full length x 0.58 ft H x 0.13 ft D, with two areas of exposed steel, 2.0 ft long, with 10% section loss (See Photo 23). (1090 - 40 FT)
- 6) Beam 5-1, left leg has a longitudinal crack, 0.13 in wide, adjacent to midspan diaphragm and extending to the bent cap, with corrosion bleed out (See Photo 24). (1130 - 5 FT)
- 8) Beam 5-12, right leg has a spall/delamination, 25.0 ft L x 0.83 ft H x 0.17 ft D, with seven exposed stirrups and one longitudinal piece of rebar (See Photo 25). (1080 - 25 FT)
- 9) Beam 6-4, right leg has a spall, 1.5 ft L x 0.50 ft W x 0.08 ft D, 15.0 ft from Bent Cap 6 (See Photo 26). (1080 - 2 FT)

## NOTES

## NEW:

- 1) The diaphragm of Beam 1-3 is delaminated, 2.08 ft W x 1.08 ft H, adjacent to the near abutment.
- 2) The midspan diaphragm of Beam 3-8 has a delamination, 0.25 ft H x 1.17 ft W, in the near face.
- 3) The midspan diaphragm of Beam 5-5 has a spall, 0.42 ft L x 0.83 ft W x 0.50 in D, in the bottom face (See Photo 27).
- 4) The midspan diaphragm of Beam 5-8 has a spall/delamination, 0.42 ft L x 0.83 ft W, in the bottom face (See Photo 27).

1080/4 -

1090/4 -

1130/4 -

**SUPERSTRUCTURE : Superstructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	331 / 4	Re Conc Bridge Railing	453	93.6	30	6.2	1	0.21	0	.	484 ft
0	1120 / 4	Efflorescence/Rust Staining	0	.	0	.	1	100	0	.	1 ft
0	1130 / 4	Cracking (RC and Other)	0	.	30	100	0	.	0	.	30 ft

**Element Inspection Notes:**

331/4 CONDITION STATE 2

## NEW:

- 1) There are vertical cracks, full height x 0.02 in W, throughout the bridge rails.(1130 - 30 FT)

CONDITION STATE 3

## NEW:

- 1) The right bridge rail has an area of corrosion bleed out in Span 4 (See Photo 28). (1120 - 1 FT)

1120/4 -

1130/4 -

**Total Number of Elements\*: 11**

\*excluding defects/protective systems

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**Inspector Recommendations**

**UNIT: 0****DECKS****ELEMENT/ENV: 12 / 4 Re Concrete Deck****ELEM CATEGORY: Decks/Slabs**

CONDITION STATE		PRIORITY
1, 2	MMS Quantity: 1 sf    Element Estimated Quantity: 1 sq.ft	3
WORK ORDER RECOMMENDATION: Approach guardrails; install rail system to meet current state standards.		
1, 2	MMS Quantity: 1 sf    Element Estimated Quantity: 1 sq.ft	3
WORK ORDER RECOMMENDATION: Object markers; install at all four bridge corners.		

**ELEMENT/ENV: 12:510:3220 / 4 Crack (Wearing Surface)****ELEM CATEGORY: Decks/Slabs**

CONDITION STATE		PRIORITY
3	MMS Quantity: 1 sf    Element Estimated Quantity: 1041 sq.ft	3
WORK ORDER RECOMMENDATION: Deck asphalt overlay; seal longitudinal cracks.		

**UNIT: 0****MISCELLANEOUS****ELEMENT/ENV: 321 / 4 Re Conc Approach Slab****ELEM CATEGORY: Other Elements**

CONDITION STATE		PRIORITY
1	MMS Quantity: 1 sf    Element Estimated Quantity: 1 sq.ft	3
WORK ORDER RECOMMENDATION: Approach roadway/slab transition; seal transvers cracks.		

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**Inspector Recommendations**

**UNIT: 0                      SUBSTRUCTURE**

**ELEMENT/ENV: 215:1080 / 4 Delamination/Spall/Patched Area**

**ELEM CATEGORY: Substructure**

CONDITION  
STATE

PRIORITY

3                      MMS Quantity: 1 mh    Element Estimated Quantity: 2 ft

3

WORK ORDER RECOMMENDATION:

Backwalls; clean and patch spalls with exposed steel.

**ELEMENT/ENV: 215:1090 / 4 Exposed Rebar**

**ELEM CATEGORY: Substructure**

CONDITION  
STATE

PRIORITY

3                      MMS Quantity: 1 mh    Element Estimated Quantity: 5 ft

3

WORK ORDER RECOMMENDATION:

Backwalls; clean and patch spalls with exposed steel and section loss.

**ELEMENT/ENV: 227:1080 / 4 Delamination/Spall/Patched Area**

**ELEM CATEGORY: Substructure**

CONDITION  
STATE

PRIORITY

2, 3                      MMS Quantity: 1 mh    Element Estimated Quantity: 1 (EA)

3

WORK ORDER RECOMMENDATION:

Pile 7-1; clean and patch spall with exposed steel.

**ELEMENT/ENV: 227:1130 / 4 Cracking (RC and Other)**

**ELEM CATEGORY: Substructure**

CONDITION  
STATE

PRIORITY

3                      MMS Quantity: 1 mh    Element Estimated Quantity: 2 (EA)

0

WORK ORDER RECOMMENDATION:

Piles 3-4 and 5-3; clean and epoxy inject cracks.

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**Inspector Recommendations**

**UNIT: 0**      **SUPERSTRUCTURE****ELEMENT/ENV: 110:1080 / 4 Delamination/Spall/Patched Area****ELEM CATEGORY: Superstructure**

CONDITION STATE		PRIORITY
3	MMS Quantity: 1 mh    Element Estimated Quantity: 27 ft	3
WORK ORDER RECOMMENDATION: Beams 2-11 4-5 and 5-12; clean and patch spalls with exposed steel.		

**ELEMENT/ENV: 110:1090 / 4 Exposed Rebar****ELEM CATEGORY: Superstructure**

CONDITION STATE		PRIORITY
3	MMS Quantity: 1 mh    Element Estimated Quantity: 40 ft	3
WORK ORDER RECOMMENDATION: Beam 4-12; clean and patch spall with exposed steel and section loss.		

**ELEMENT/ENV: 110:1130 / 4 Cracking (RC and Other)****ELEM CATEGORY: Superstructure**

CONDITION STATE		PRIORITY
3	MMS Quantity: 1 mh    Element Estimated Quantity: 5 ft	3
WORK ORDER RECOMMENDATION: Beam 5-1; epoxy inject crack.		

**Structure Notes**

Bridge entered 3/16/2022 1:59:08 PM by userid KNIEIDS

**INSPECTION NOTES:**      **IUVI**      **2/28/2022**

Sufficiency Rating Calculation Accepted by KNIEIAN at 4/28/2022 3:24:15 PM  
UW TANK = 2/28/22

TRAFFIC RESTRICTIONS: The bridge is not posted. Based on our initial (2022) field inspection and the 2022 load capacity analysis, posting restrictions are not considered necessary at this time.

The load rating currently filed in the Department's Electronic Document Management System, sealed on 4/28/2022 by David M. Stump, Jr., P.E., was reviewed by David M. Stump, Jr., P.E., and found to be complete and applicable.

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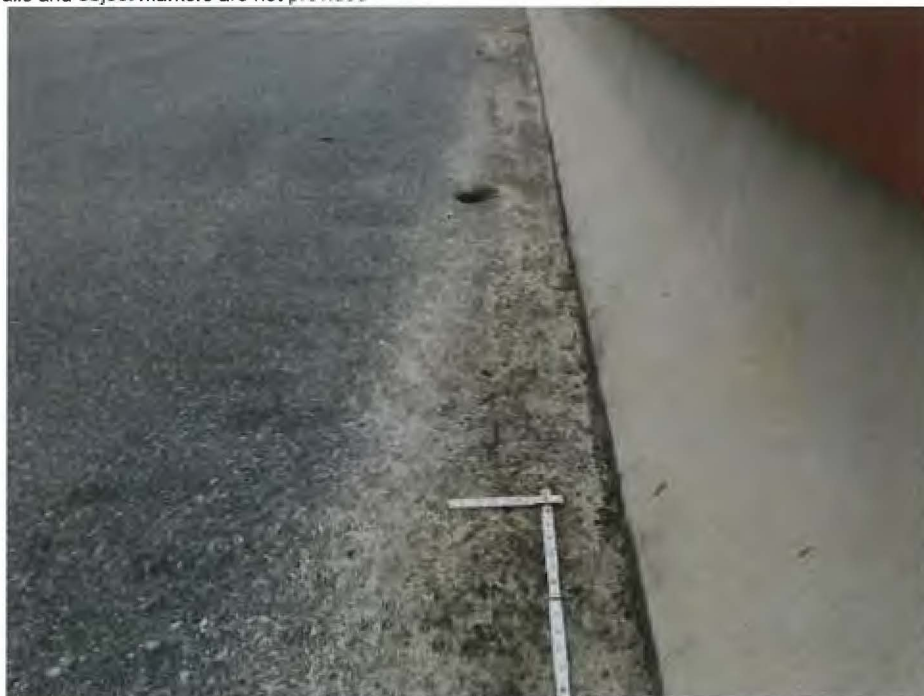
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**12 - Re Concrete Deck**

Photo 1: Approach guardrails and object markers are not provided



**12 - Re Concrete Deck**

Photo 2: Dirt and debris along deck shoulders

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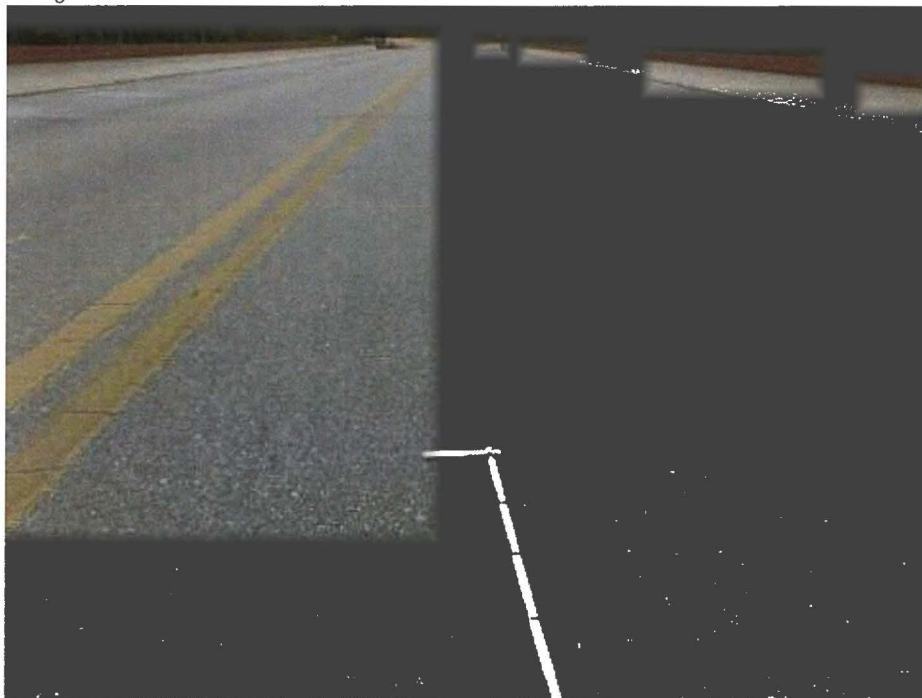
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**12 - Re Concrete Deck**

Photo 3: Typical utility pipe along deck underside



**12 - Re Concrete Deck (510 - Wearing Surfaces)**

Photo 4: Typical longitudinal crack in deck asphalt overlay

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**321 - Re Conc Approach Slab**

Photo 5: Typical crack at approach roadway/slab transition



**8475 - R/Conc Walls**

Photo 6: Typical spall in near left wingwall pile

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**8475 - R/Conc Walls**

Photo 7: Spall in near right wingwall pile



**215 - Re Conc Abutment**

Photo 8: Spall with exposed steel in near backwall

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215 - Re Conc Abutment

Photo 9: Spall with exposed steel with section loss of near backwall



215 - Re Conc Abutment

Photo 10: Spall with exposed steel with section loss of far backwall

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**215 - Re Conc Abutment**

Photo 11: Spall with exposed steel in far backwall



**215 - Re Conc Abutment**

Photo 12: Spall on Abutment Cap 7

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**227 - Re Conc Pile**

Photo 13: Typical spall on pile corner



**227 - Re Conc Pile**

Photo 14: Typical crack in pile

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227 - Re Conc Pile

Photo 15: Typical crack with corrosion bleed out on pile



227 - Re Conc Pile

Photo 16: Typical failing patch on pile

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227 - Re Conc Pile

Photo 17: Spall with exposed steel on Pile 7-1



234 - Re Conc Pier Cap

Photo 18: Crack in left end of Bent Cap 6

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109 - Pre Opn Conc Girder/Beam

Photo 19: Spall with exposed steel in Beam 1-1



109 - Pre Opn Conc Girder/Beam

Photo 20: Spall on Beam 1-6

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**109 - Pre Opn Conc Girder/Beam**

Photo 21: Spall in bottom face of Beam 1-8



**109 - Pre Opn Conc Girder/Beam**

Photo 22: Typical spall with exposed steel in beam

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109 - Pre Opn Conc Girder/Beam

Photo 23: Typical spall/delamination with exposed steel and section loss in Beam 4-12



109 - Pre Opn Conc Girder/Beam

Photo 24: Longitudinal crack with corrosion bleed out along Beam 5-1

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**109 - Pre Opn Conc Girder/Beam**

Photo 25: Typical spall/delamination with exposed steel along Beam 5-12



**109 - Pre Opn Conc Girder/Beam**

Photo 26: Spall in bottom face of Beam 6-4

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Structure ID: 460167

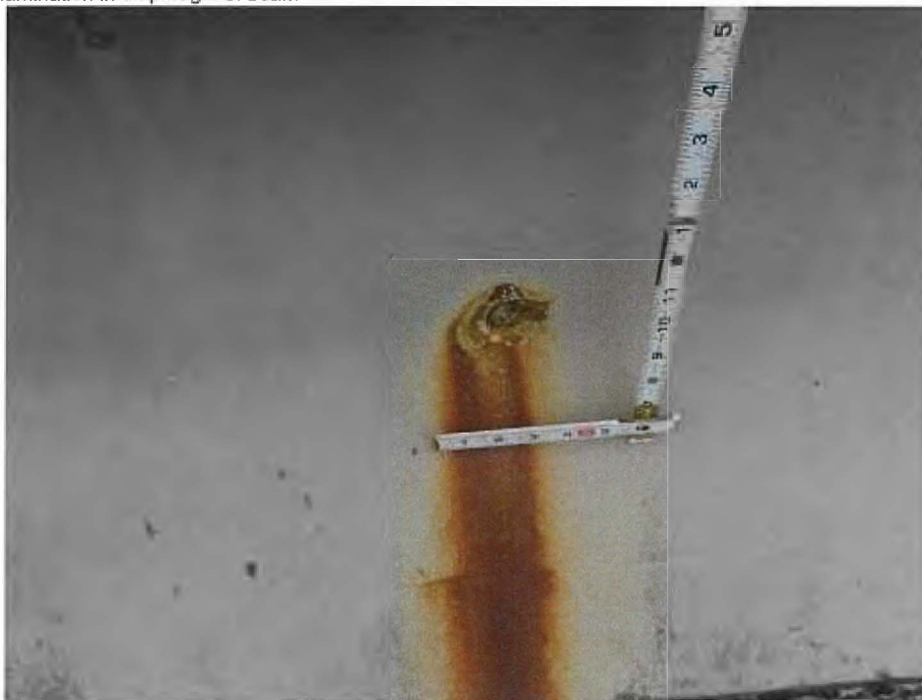
DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI



**109 - Pre Opn Conc Girder/Beam**

Photo 27: Typical spall/delamination in diaphragm of beam



**331 - Re Conc Bridge Railing**

Photo 28: Area of corrosion bleed out in right bridge rail in Span 4

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FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM  
Inspection/CIDR/Bridge Profile Report  
Inspection

Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI



Looking on With Station

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FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM  
Inspection/CIDR/Bridge Profile Report  
Inspection

Structure ID: 460167  
DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

BRIDGE LOCATION MAP

BRIDGE NO: 460167  
COUNTY: Walton  
FACILITY CARRIED: Wild Heron Way  
FEATURE INTERSECTED: Powell Lake  
LOCATION: 0.3 Miles N of SR30A/US98



Note: This structure is located on a dead end route; a detour route is not applicable.

Location Map

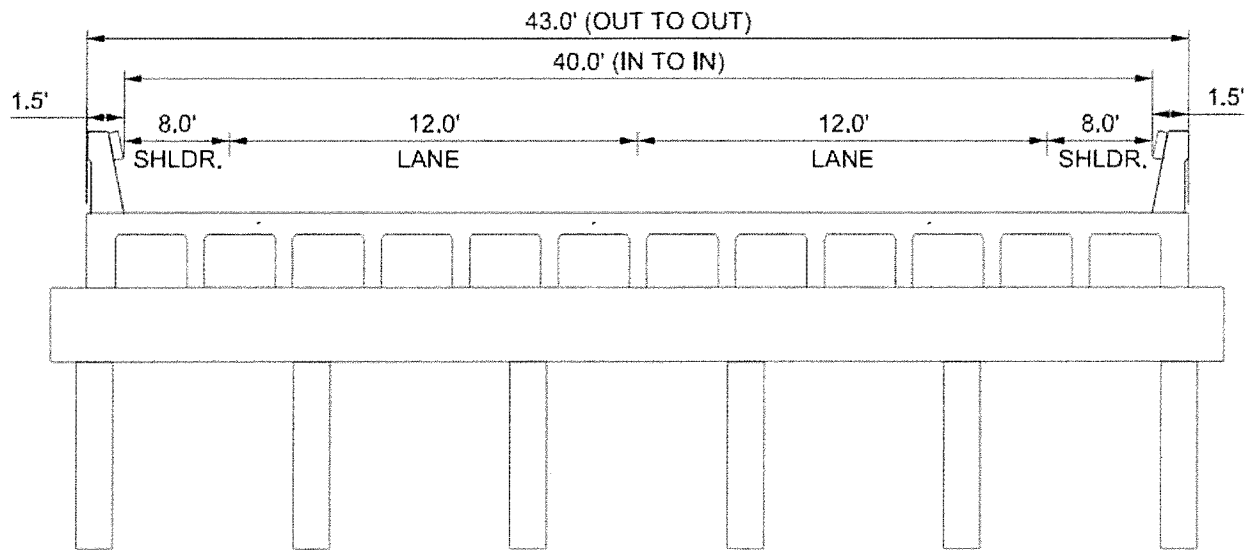
This report contains information relating to the physical security of a structure and depictions of the structure. This information is confidential and exempt from public inspection pursuant to sections 119.071(3)(a) and 119.071(3)(b), Florida Statutes. Only the cover page of this report may be inspected and copied.

FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM  
Inspection/CIDR/Bridge Profile Report  
Inspection

Structure ID: 460167  
DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

BRIDGE NO. 460167  
BAY COUNTY



TYPICAL SECTION

Inventory Sketch 1

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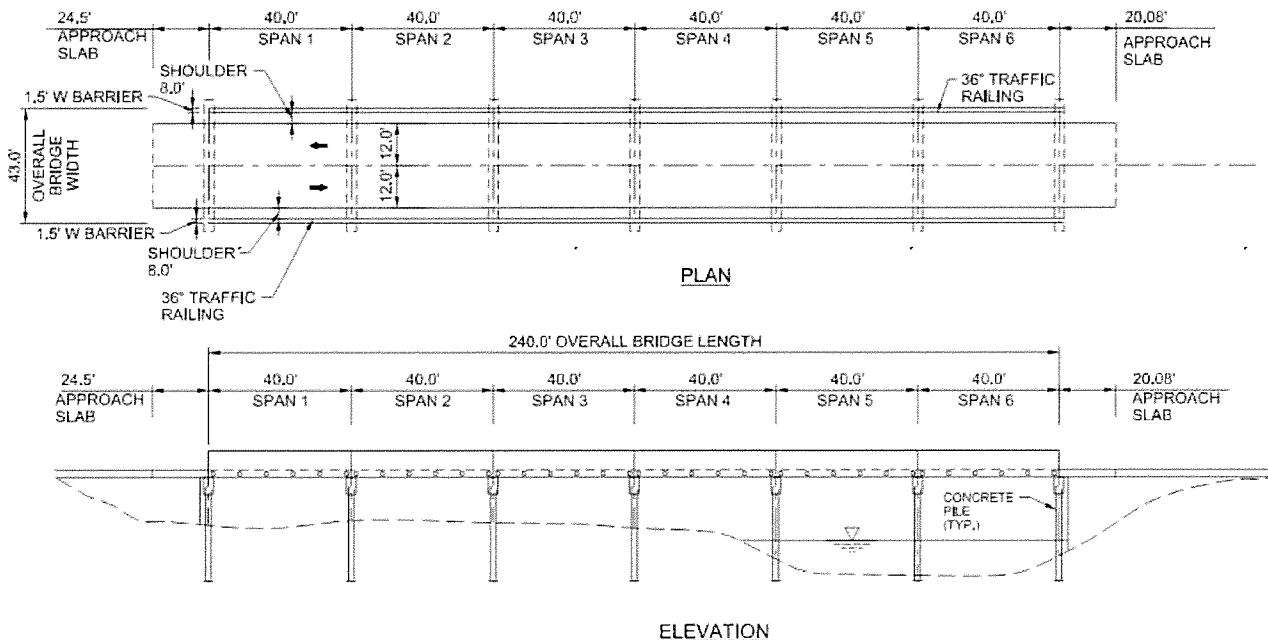
FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM  
Inspection/CIDR/Bridge Profile Report  
Inspection

Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

BRIDGE NO. 460167  
BAY COUNTY



Inventory Sketch 2

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## FLORIDA DEPARTMENT OF TRANSPORTATION BRIDGE MANAGEMENT SYSTEM Inspection/CIDR/Bridge Profile Report Inspection


Structure ID: 460167

DISTRICT: D3 - Chipley

INSPECTION DATE: 2/28/2022 IUVI

Bridge No.	460167	Analysis Method:	LRFR-LRFD	<b>FDOT Bridge Load Rating Summary Form (Page 1 of 1)</b>	
Location	Wild Heron Way over Powell Lake				
Description	6 Span, Precast Reinforced Concrete Channel Beams (40.0' Span)				

Rating Type	Rating Type	Design Weight (kips)	Material/Steel/Concrete	Design Load Factor	Live Load Factor	Wind Load Factor (k/ft)	Rating Factor	Span No.	Span No. Location/Remarks	Rating
Level	Vehicle	Weight	Member Type	Limit	DC	LL	LLDF	RF	Spanning Location	RATING
Inventory	HL93	36	Reinf. Concrete	Strength, Moment	1.75/0.90	1.75	0.340	1.210	Span 10, 50% Span	43.6
Operating	HL93	36	Reinf. Concrete	Strength, Moment	1.25/0.90	1.35	0.340	1.570	Span 10, 50% Span	56.5
Permit	FL120	60	Reinf. Concrete	Strength, Moment	1.25/0.90	1.35	0.340	1.320	Span 10, 50% Span	73.2
Normal Max. Load	FL120	60	Reinf. Concrete	Strength, Moment	1.25/0.90	1.35	0.340	1.220	Span 10, 50% Span	73.2
Legal	SU2	17	Member Type	Limit Type	NA	NA				-1
	SU3	33	Member Type	Limit Type	NA	NA				-3
	SU4	35	Member Type	Limit Type	NA	NA				1
	C3	28	Member Type	Limit Type	NA	NA				1
	C4	34.7	Member Type	Limit Type	NA	NA				-3
	C5	40	Member Type	Limit Type	NA	NA				-1
Emergency Vehicle (EV)	EV2	28.75	Member Type	Limit Type	NA	NA				-3
	EV3	43	Member Type	Limit Type	NA	NA				-1

Original Design Load	HS20 or HS20-S16-44	Performed by:	A. Allen	Date:	04/30/22
Rating Type, Analysis	LRFR-LRFD	Checked by:	D. Diaz, DJ	Date:	04/15/22
Distribution Method	AASHTO Formula	 <p style="font-size: small;">David M Stump Jr. 3822-04-88 18-12-01-04100</p> <p style="font-size: x-small;">1560 Capital Circle NW, Suite 3 Tallahassee, FL 32307 dstump@conioneng.com</p>			
Impact Factor	33.0% (ade loading)				
FL120 Gov. Span Length	40.0 (feet)				
Minimum Span Length	40.0 (feet)				
Recommended Posting	All Above legal loads. Posting Not Required.				
Recommended SU Posting	99 (tons)				
Recommended C Posting	99 (tons)				
Recommended STS Posting	99 (tons)				
Owner	26 Private (other than railroad)				
Location	Neither interstate traffic nor within 1 mile of interstate. No posting is not recommended. The EAST side of the bridge.				
CV Posting		Comments: Load rating based on AASHTO standard index plans for Precast Reinforced Channel Beams which were used in design per manufacturer, CONECLUH.			
Floor Beam Present?	No				
Segmental Bridge?	No				
Project No. & Reason	22485017211 New Bridge				
Plans Status	Design or Construction				

This is a 2022 summary listing the FDOT Bridge Load Rating Manual (BLRM), and the FDOT BMS Loading Manual. \*Recommended SU Posting levels for Florida SU loads are currently defined by AASHTO for more, see AASHTO Design 1.

File: g:\m\m\m\m\m\LoadRating\Summary

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**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM**

REPORT ID: INSP005

Inspection/CIDR/Bridge Profile Report

Structure ID: 460167

CIDR

DATE PRINTED: 4/28/2022

**Description**

**Structure Unit Identification**

Bridge/Unit Key: 460167 1  
 Structure Name:  
 Description: MAIN SPAN 1  
 Type: M - Main

**Roadway Identification**

NBI Structure No (8): 460167  
 Position/Prefix (5): 1 - Route On Structure  
 Kind Hwy (Rte Prefix): 5 City Street  
 Design Level of Service: 1 Mainline  
 Route Number/Suffix: 00000 / 0 N/A (NBI)  
 Feature Intersect (6): Powell Lake  
 Critical Facility: Not Defense-crit  
 Facility Carried (7): Wild Heron Way  
 Mile Point (11): 0.3  
 Latitude (16): 030d15'53.0" Long (17): 085d57'14.0"

**Roadway Traffic and Accidents**

Lanes (28): 2 Medians: 0 Speed: 35 mph  
 ADT Class: 1 ADT Class 1  
 Recent ADT (29): 500 Year (30): 2020  
 Future ADT (114): 550 Year (115): 2040  
 Truck % ADT (109): 0  
 Detour Length (19): 99.0 mi  
 Detour Speed: 35 mph  
 Accident Count: Rate:

**Roadway Classification**

Nat. Hwy Sys (104): 0 Not on NHS  
 National base Net (12): 0 - Not on Base Network  
 LRS Inventory Rte (13a): 46 000 000 Sub Rte (13b): 00  
 Functional Class (26): 19 Urban Local  
 Federal Aid System: OFF  
 Defense Hwy (100): 0 Not a STRAHNET hwy  
 Direction of Traffic (102): 2 2-way traffic  
 Emergency:

**Roadway Clearances**

Vertical (10): 99.99 ft Appr. Road (32): 43 ft  
 Horiz. (47): 43 ft Roadway (51): 43 ft  
 Truck Network (110): 0 Not part of natl netwo  
 Toll Facility (20): 3 On free road  
 Fed. Lands Hwy (105): 0 N/A (NBI)  
 School Bus Route:   
 Transit Route:

**NBI Project Data**

Proposed Work (075A): Not Applicable (P)  
 Work To Be Done By (075B): Not Applicable (P)  
 Improvement Length (076): 0 ft

Improvement Cost (094): \$ 0.00  
 Roadway Improvement Cost (095): \$ 0.00  
 Total Cost (096): \$ 0.00  
 Year of Estimate (097):

**NBI Rating**

Channel (61): 6 Bank Slumping  
 Deck (58): 7 Good  
 Superstructure (59): 5 Fair  
 Substructure (60): 5 Fair

Culvert (62): N N/A (NBI)  
 Waterway (71): 7 Above Minimum  
 Unrepaired Spalls:  
 Review Required:

**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM**

REPORT ID: INSP005

Inspection/CIDR/Bridge Profile Report

Structure ID: 460167

CIDR

DATE PRINTED: 4/28/2022

**Structure Identification**

Admin Area: Bay County  
 District (2): D3 - Chipley  
 County (3): (46)Bay  
 Place Code (4): No city involved  
 Location (9): 0.3 Miles N of SR30A/US98  
 Border Br St/Reg (98): Not Applicable (P) Share: 0 %  
 Border Struct No (99):  
 FIPS State/Region (1): 12 Florida Region 4-Atlanta  
 NBIS Bridge Len (112): Y - Meets NBI Length  
 Parallel Structure (101): No || bridge exists  
 Temp. Structure (103): Not Applicable (P)  
 Maint. Resp. (21): 26 Private(nonRailroad)  
 Owner (22): 26 Private(nonRailroad)  
 Historic Signif. (37): 5 Not eligible for NRHP

**Structure Type and Material**

Curb/Sidewalk (50): Left: 1.17 ft Right: 1.17 ft  
 Bridge Median (33): 0 No median  
 Main Span Material (43A): 1 Reinforced Concrete  
 Appr Span Material (44A): Not Applicable (P)  
 Main Span Design (43B): 22 Channel Beam  
 Appr Span Design (44B): Not Applicable (P)

**Geometrics**

Spans in Main Unit (45): 6  
 Approach Spans (46): 0  
 Length of Max Span (48): 40 ft  
 Structure Length (49): 240 ft  
 Total Length: 280 ft  
 Deck Area: 10406 sqft  
 Structure Flared (35): 0 No flare

**Age and Service**

Year Built (27): 2022  
 Year Reconstructed (106): 0  
 Type of Service On (42a): 1 Highway  
 Under (42b): 5 Waterway  
 Fracture Critical Details: Not Applicable

**Deck Type and Material**

Deck Width (52): 43 ft  
 Skew (34): 0 deg  
 Deck Type (107): 1 Concrete-Cast-in-Place  
 Surface (108): 0 None  
 Membrane: 0 None  
 Deck Protection: None

**Appraisal****Structure Appraisal**

Open/Posted/Closed (41): A Open, no restriction  
 Deck Geometry (68): 9 Above Desirable Crit  
 Underclearances (69): N Not applicable (NBI)  
 Approach Alignment (72): 8-No Speed Red thru Curv  
 Bridge Railings (36a): 1 Meets Standards  
 Transitions (36b): 1 Meets Standards  
 Approach Guardrail (36c): 1 Meets Standards  
 Approach Guardrail Ends (36d): 1 Meets Standards  
 Scour Critical (113): U Unknown Foundation

**Minimum Vertical Clearance**

Over Structure (53): 99.99 ft  
 Under (reference) (54a): N Feature not hwy or RR  
 Under (54b): 0 ft

**Navigation Data**

Navigation Control (38): Permit Not Required  
 Nav Vertical Clr (39): 0 ft  
 Nav Horizontal Clr (40): 0 ft  
 Min Vert Lift Clr (116): 0 ft  
 Pier Protection (111): Not Applicable (P)

**NBI Condition Rating**

Sufficiency Rating: 79.7  
 Health Index: 86.97  
 Structural Eval (67): 5 Above Min Tolerable  
 Deficiency: Not Deficient

**Minimum Lateral Underclearance**

Reference (55a): N Feature not hwy or RR  
 Right Side (55b): 0 ft  
 Left Side (56): 0 ft

**Schedule****Current Inspection**

Inspection Date: 02/28/2022  
 Inspector: KNIEIJO - Joseph Stewart  
 Bridge Group: CA058  
 Alt. Bridge Group:  
 Primary Type: Regular NBI  
 Review Required:

**Next Inspection Date Scheduled**

NBI: 02/28/2024  
 Element: 02/28/2024  
 Fracture Critical:  
 Underwater: 02/28/2024  
 Other/Special:  
 Inventory Photo Update Due: 02/28/2032

**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM**

REPORT ID: INSP005

**Inspection/CIDR/Bridge Profile Report**

Structure ID: 460167

CIDR

DATE PRINTED: 4/28/2022

**Schedule Cont.**

**Inspection Types**

**Performed**

NBI  Element  Fracture Critical  Underwater  Other Special

**Inspection Intervals**

**Required (92)**

**Frequency (92)**

**Last Date (93)**

**Inspection Resources**

Fracture Critical	<input type="checkbox"/>	mos			Crew Hours: 13
Underwater	<input checked="" type="checkbox"/>	24 mos	02/28/2022		Flagger Hours: 0
Other Special	<input type="checkbox"/>	mos			Helper Hours: 0
NBI		24 mos (91)	02/28/2022 (90)		Snooper Hours: 0
					Special Crew Hours: 0
					Special Equip Hours: 0

**Bridge Related**

**General Bridge Information**

Parallel Bridge Seq: 0000

Channel Depth: 6.9 ft

Radio Frequency:

Phone Number:

Exception Date:

Exception Type:

Accepted By Maint:

Warranty Expiration: 00/00/0000

Performance Rating: Fair

Permitted Utilities: Power  Water  Gas  Fiber Optic  Sewage  Other

Bridge Rail 1: Concrete jersey type

Bridge Rail 2: Not applicable-No rail

Electrical Devices: No electric service

Culvert Type: Not applicable

Maintenance Yard: Not FDOT Maintained

FIHS ON / OFF: No Routes on FIHS

Previous Structure:

2nd Previous Structure:

Replacement Structure:

**Bridge Load Rating Information**

Inventory Type (065): 3 LRFR Load &amp; Res. Fact

Operating Type (063): 3 LRFR Load &amp; Res. Fact

Original Design Load (031): 5 MS 18 (HS 20)

Date: 04/28/2022

Initials: DMS

Load Rating Rev. Recom.: No

Load Rating Plans Status: Design or Construction

Inventory Rating (066): 43.6 tons

Operating Rating (064): 56.5 tons

FL120 Permit Rating: 73.2 tons

HS20/FL120 Max Span Rating: 73.2 tons

Dynamic Impact in Percent: 33 %

Governing Span Length: 40.0 ft

Minimum Span Length: 40.0 ft

Distribution Method: AASHTO formula

Load Rating Notes:

**LEGAL LOADS**

SU2: -1.0 tons

SU3: -1.0 tons

SU4: -1.0 tons

C3: -1.0 tons

C4: -1.0 tons

C5: -1.0 tons

ST5: -1.0 tons

Posting (070): 5 At/Above Legal Loads

Open/Posted/Closed (041): A Open, no restriction

**FLOOR BEAM (FB)**

FB Present: No

FB Span Length, Gov: 0.0 ft

FB Spacing, Gov: 0.0 ft

FB OPR Rating: 0.0 tons

FB SU4 OPR Rating: 0.0 tons

FB FL120 Rating: 0.0 tons

**POSTING**

Recom. SU Posting: 99 tons

Recom. C Posting: 99 tons

Recom. ST5 Posting: 99 tons

Actual SU Posting: 99 tons

Actual C Posting: 99 tons

Actual ST5 Posting: 99 tons

Actual Blanket Posting: 99 tons

Emergency Vehicle: 2 EV unrestricted

**SEGMENTAL (SEG)**

SEG Wing-Span: -1.0 ft

SEG Web-to-Web Span: -1.0 ft

SEG Transverse HL93 Operating: -1.00 RF

**Bridge Scour and Storm Information**

Pile Driving Record: Unknown

Foundation Type: Unknown

Mode of Flow: Riverine

Rating Scour Eval: Unknown

Highest Scour Eval: No phase completed

Scour Evaluation Method: Unknown – Eval Not Comp

Scour Recommended I: Unknown

Scour Recommended II: Unknown

Scour Recommended III: Unknown

Scour Elevation: 999 ft

Action Elevation: 999 ft

Storm Frequency: 999

**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM**

REPORT ID: INSP005

Inspection/CIDR/Bridge Profile Report

Structure ID: 460167

CIDR

DATE PRINTED: 4/28/2022

**Elements**

Inspection Date: 02/28/2022 IUVI

**DECKS : Decks/Slabs**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	12 / 4	Re Concrete Deck	10197	97.99	209	2.01	0	.	0	.	10406 sq.ft
0	1090 / 4	Exposed Rebar	0	.	1	100	0	.	0	.	1 sq.ft
0	1130 / 4	Cracking (RC and Other)	0	.	208	100	0	.	0	.	208 sq.ft
0	510 / 4	Wearing Surfaces	9365	90	0	.	1041	10	0	.	10406 sq.ft
0	3220 / 4	Crack (Wearing Surface)	0	.	0	.	1041	100	0	.	1041 sq.ft

**DECKS : Joints**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	301 / 4	Pourable Joint Seal	301	100	0	.	0	.	0	.	301 ft

**MISCELLANEOUS : Channel**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	8290 / 4	Channel	1	100	0	.	0	.	0	.	1 (EA)

**MISCELLANEOUS : Other Elements**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	321 / 4	Re Conc Approach Slab	1002	100	0	.	0	.	0	.	1002 sq.ft
0	510 / 4	Wearing Surfaces	1002	100	0	.	0	.	0	.	1002 sq.ft

**MISCELLANEOUS : Other Elements**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	8475 / 4	R/Conc Walls	61	95.31	0	.	3	4.69	0	.	64 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	3	100	0	.	3 ft

**SUBSTRUCTURE : Substructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	215 / 4	Re Conc Abutment	66	76.74	12	13.95	8	9.3	0	.	86 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	3	100	0	.	3 ft
0	1090 / 4	Exposed Rebar	0	.	0	.	5	100	0	.	5 ft
0	1130 / 4	Cracking (RC and Other)	0	.	12	100	0	.	0	.	12 ft

**SUBSTRUCTURE : Substructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	227 / 4	Re Conc Pile	0	.	35	71.43	14	28.57	0	.	49 (EA)
0	1080 / 4	Delamination/Spall/Patched Area	0	.	35	85.37	6	14.63	0	.	41 (EA)
0	1130 / 4	Cracking (RC and Other)	0	.	0	.	8	100	0	.	8 (EA)

**SUBSTRUCTURE : Substructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	234 / 4	Re Conc Pier Cap	182	84.65	23	10.7	10	4.65	0	.	215 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	1	100	0	.	0	.	1 ft
0	1130 / 4	Cracking (RC and Other)	0	.	22	68.75	10	31.25	0	.	32 ft

**SUPERSTRUCTURE : Bearings**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
----------	----------	-------------	------	----	------	----	------	----	------	----	-------

**FLORIDA DEPARTMENT OF TRANSPORTATION  
BRIDGE MANAGEMENT SYSTEM**

REPORT ID: INSP005

**Inspection/CIDR/Bridge Profile Report**

Structure ID: 460167

CIDR

DATE PRINTED: 4/28/2022

0	310 / 4	Elastomeric Bearing	288	100	0	.	0	.	0	.	288 each
---	---------	---------------------	-----	-----	---	---	---	---	---	---	----------

**SUPERSTRUCTURE : Superstructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	110 / 4	Re Conc Opn Girder/Beam	2767	97.29	0	.	77	2.71	0	.	2844 ft
0	1080 / 4	Delamination/Spall/Patched Area	0	.	0	.	32	100	0	.	32 ft
0	1090 / 4	Exposed Rebar	0	.	0	.	40	100	0	.	40 ft
0	1130 / 4	Cracking (RC and Other)	0	.	0	.	5	100	0	.	5 ft

**SUPERSTRUCTURE : Superstructure**

Str Unit	Elem/Env	Description	Qty1	%1	Qty2	%2	Qty3	%3	Qty4	%4	T Qty
0	331 / 4	Re Conc Bridge Railing	453	93.6	30	6.2	1	0.21	0	.	484 ft
0	1120 / 4	Efflorescence/Rust Staining	0	.	0	.	1	100	0	.	1 ft
0	1130 / 4	Cracking (RC and Other)	0	.	30	100	0	.	0	.	30 ft

**Total Number of Elements\*: 11**

\*excluding defects/protective systems

**Inspection Information****Inspection Date:** 02/28/2022**Type:** Regular NBI**Inspector:** KNIEIJO - Joseph Stewart**Inspection Notes:** Sufficiency Rating Calculation Accepted by KNIEIAN at 4/28/2022 3:24:15 PM  
UW TANK = 2/28/22

TRAFFIC RESTRICTIONS: The bridge is not posted. Based on our initial (2022) field inspection and the 2022 load capacity analysis, posting restrictions are not considered necessary at this time.

The load rating currently filed in the Department's Electronic Document Management System, sealed on 4/28/2022 by David M. Stump, Jr., P.E., was reviewed by David M. Stump, Jr., P.E., and found to be complete and applicable.

**Structure Notes**

Bridge entered 3/16/2022 1:59:08 PM by userid KNIEIDS

**Schedule Notes**

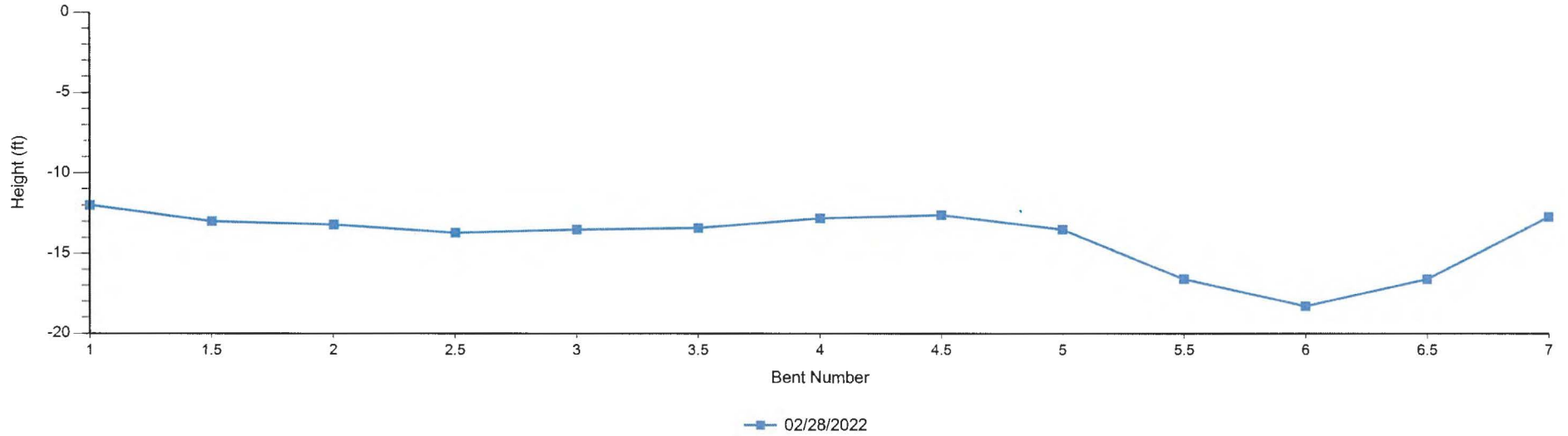
# FLORIDA DEPARTMENT OF TRANSPORTATION BRIDGE MANAGEMENT SYSTEM

## Inspection/CIDR/Bridge Profile Report

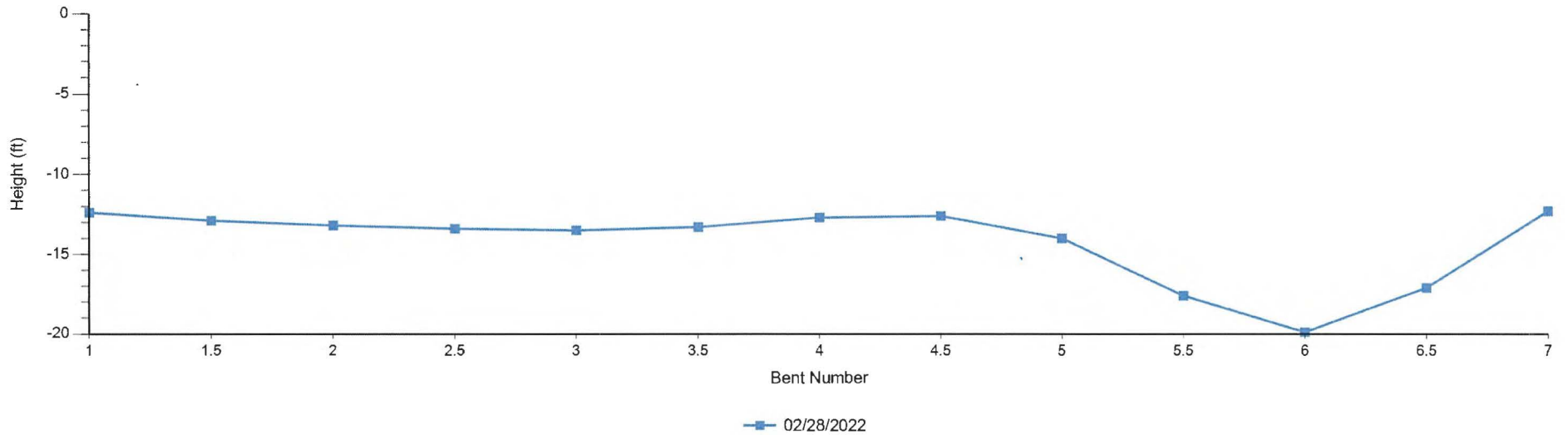
### Bridge Profile

DATE PRINTED: 4/28/2022 4:30:35 PM

#### Left Profile by Inspection



#### Right Profile by Inspection



**FLORIDA DEPARTMENT OF TRANSPORTATION BRIDGE MANAGEMENT SYSTEM**  
**Inspection/CIDR/Bridge Profile Report**  
**Bridge Profile**

**Profile Data - Numerical Summary**

Inspection Date and Key: 2/28/2022	IUVI	Bent #	Left Height	Right Height	(All Heights are in Feet)
		1	12.00	12.40	
		1.5	13.00	12.90	
		2	13.20	13.20	
		2.5	13.70	13.40	
		3	13.50	13.50	
		3.5	13.40	13.30	
		4	12.80	12.70	
		4.5	12.60	12.60	
		5	13.50	14.00	
		5.5	16.60	17.60	
		6	18.30	19.90	
		6.5	16.60	17.10	
		7	12.70	12.30	

**Air Temp:** 62

**Profile Notes:**

Waterway Measurements: Top of rail to waterline at Bent 6 = 13.0 ft Left and Right.  
 Groundline Measurements from top of rail.



**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**UNAUDITED  
FINANCIAL  
STATEMENTS**

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
FINANCIAL STATEMENTS  
UNAUDITED  
AUGUST 31, 2022**

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
BALANCE SHEET  
GOVERNMENTAL FUNDS  
AUGUST 31, 2022**

	General Fund	Debt Service Fund Series 2012	Total Funds
<b>ASSETS</b>			
Operating accounts			
BB&T	\$ 156,815	\$ -	\$ 156,815
Wells Fargo - operating*	571,489	-	571,489
Centennial Bank	257,281	-	257,281
FineMark			
Designated - stormwater compliance	127,064	-	127,064
Undesignated	121,747	-	121,747
FineMark - ICS	738,533	-	738,533
Investments			
Revenue	-	332,996	332,996
Reserve	-	198,912	198,912
Prepayment A	-	7,809	7,809
Due from governmental funds			
General fund	-	9,103	9,103
Due from other	9,850	-	9,850
Deposits	2,075	-	2,075
Total assets	<u>\$ 1,984,854</u>	<u>\$ 548,820</u>	<u>\$ 2,533,674</u>
<b>LIABILITIES &amp; FUND BALANCES</b>			
Liabilities:			
Debt service	\$ 9,103	\$ -	\$ 9,103
Total liabilities	<u>9,103</u>	<u>-</u>	<u>9,103</u>
Fund balances:			
Committed			
Disaster	250,000	-	250,000
District bridge projects	100,000	-	100,000
Road project 2022	150,000	-	150,000
Stormwater system upgrades	50,000	-	50,000
Restricted for:			
Debt service	-	548,820	548,820
Assigned to:			
3 months working capital	248,575	-	248,575
Unassigned	1,177,176	-	1,177,176
Total fund balances	<u>1,975,751</u>	<u>548,820</u>	<u>2,524,571</u>
Total liabilities and fund balances	<u>\$ 1,984,854</u>	<u>\$ 548,820</u>	<u>\$ 2,533,674</u>

\*Accounts not reconciled as statement not received prior to agenda preparation date.

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
STATEMENT OF REVENUES, EXPENDITURES,  
AND CHANGES IN FUND BALANCES  
FOR THE PERIOD ENDED AUGUST 31, 2022**

	Current Month	Year to Date	Budget	% of Budget
<b>REVENUES</b>				
	\$ -	\$ 678,060	\$ 670,801	101%
Interest & miscellaneous	142	819	1,000	82%
Revenue certificates	-	27,964	-	N/A
Total revenues	<u>142</u>	<u>706,843</u>	<u>671,801</u>	105%
<b>EXPENDITURES</b>				
<b>Administrative</b>				
Supervisors	1,077	9,258	5,000	185%
Management	2,596	28,557	31,153	92%
Accounting	918	10,094	11,012	92%
Assessment roll prep	1,209	13,301	14,510	92%
Audit	-	7,500	7,500	100%
Legal	80	3,660	12,000	31%
Engineering	-	3,300	13,280	25%
Postage	199	1,130	1,775	64%
Telephone	88	962	1,050	92%
Website maintenance	-	705	750	94%
Insurance	-	6,960	7,500	93%
Printing and binding	125	1,375	1,500	92%
Legal advertising	331	837	2,500	33%
Other current charges	23	652	1,200	54%
Office supplies	-	-	500	0%
Special district annual fee	-	175	175	100%
Trustee	-	-	7,431	0%
Arbitrage	-	750	1,200	63%
Dissemination agent	83	917	1,000	92%
ADA website compliance	210	210	210	100%
Total administrative	<u>6,939</u>	<u>90,343</u>	<u>121,246</u>	75%

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
STATEMENT OF REVENUES, EXPENDITURES,  
AND CHANGES IN FUND BALANCES  
FOR THE PERIOD ENDED AUGUST 31, 2022**

	Current Month	Year to Date	Budget	% of Budget
<b>Security</b>				
Contractual rangers	12,032	122,769	153,000	80%
Total security	<u>12,032</u>	<u>122,769</u>	<u>153,000</u>	80%
<b>Lake wetland &amp; upland monitoring</b>				
Mitigation and monitoring				
Prescribed fires and gyro mulching	5,300	30,010	46,050	65%
Ecologist	-	39,155	57,980	68%
Total lake wetland & upland monitoring	<u>5,300</u>	<u>69,165</u>	<u>104,030</u>	66%
<b>Roadway services</b>				
Roadway repairs and maintenance	-	17,380	63,700	27%
Roadway resurfacing	-	-	400,000	0%
Bridge repairs and maintenance	-	19,775	50,000	40%
Total roadway services	<u>-</u>	<u>37,155</u>	<u>513,700</u>	7%
<b>Stormwater management</b>				
Operations	-	-	17,250	0%
Pond aeration	376	21,667	30,000	72%
Electricity - lift stations	79	430	600	72%
Stormwater system repairs	40,555	61,005	18,000	339%
Total stormwater management	<u>41,010</u>	<u>83,102</u>	<u>65,850</u>	126%
<b>Other charges</b>				
Tax collector	-	13,561	13,975	97%
Total other charges	<u>-</u>	<u>13,561</u>	<u>13,975</u>	97%
Total expenditures	<u>65,281</u>	<u>416,095</u>	<u>971,801</u>	43%
Excess/(deficiency) of revenues over/(under) expenditures	(65,139)	290,748	(300,000)	
Fund balances - beginning	<u>2,040,890</u>	<u>1,685,003</u>	<u>1,511,302</u>	
Fund balances - ending				
Committed				
Disaster	250,000	250,000	250,000	
District bridge projects	100,000	100,000	100,000	
Road projects	150,000	150,000	150,000	
Storm system upgrades	50,000	50,000	50,000	
Assigned				
3 months working capital	248,575	248,575	248,575	
Unassigned	1,177,176	1,177,176	412,727	
Fund balances - ending	<u><u>\$1,975,751</u></u>	<u><u>\$1,975,751</u></u>	<u><u>\$1,211,302</u></u>	

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
STATEMENT OF REVENUES, EXPENDITURES,  
AND CHANGES IN FUND BALANCES  
FOR THE PERIOD ENDED AUGUST 31, 2022**

	Current Month	Year to Date	Budget	% of Budget
<b>REVENUES</b>				
Assessment levy	\$ -	\$ 434,663	\$ 430,016	101%
Special assessment - direct bill	-	360	-	N/A
Interest	479	892	-	N/A
Assessment prepayments	7,809	20,409	-	N/A
Total revenues	<u>8,288</u>	<u>456,324</u>	<u>430,016</u>	106%
<b>Debt service</b>				
Principal	-	215,000	215,000	100%
Principal prepayment	-	50,000	-	N/A
Interest	-	177,606	177,881	100%
Total debt service	<u>-</u>	<u>442,606</u>	<u>392,881</u>	113%
<b>Other charges</b>				
Tax collector	-	8,693	8,959	97%
Total other charges	<u>-</u>	<u>8,693</u>	<u>8,959</u>	97%
Total expenditures	<u>-</u>	<u>451,299</u>	<u>401,840</u>	112%
Excess/(deficiency) of revenues over/(under) expenditures	8,288	5,025	28,176	
Fund balance - beginning	540,532	543,795	573,429	
Fund balance - ending	<u>\$ 548,820</u>	<u>\$ 548,820</u>	<u>\$ 601,605</u>	

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
AMORTIZATION SCHEDULE - SERIES 2012  
\$5,160,000**

Date	Principal	Interest Rate	Interest	Total Principal & Interest
11/01/2021	215,000.00	5.250%	92,912.50	307,912.50
05/01/2022	-	-	87,268.75	87,268.75
04/30/2022	225,000.00	5.250%	87,268.75	312,268.75
05/01/2023	-	-	81,362.50	81,362.50
11/01/2023	235,000.00	5.750%	81,362.50	316,362.50
05/01/2024	-	-	74,606.25	74,606.25
11/01/2024	250,000.00	5.750%	74,606.25	324,606.25
05/01/2025	-	-	67,418.75	67,418.75
11/01/2025	265,000.00	5.750%	67,418.75	332,418.75
05/01/2026	-	-	59,800.00	59,800.00
11/01/2026	280,000.00	5.750%	59,800.00	339,800.00
05/01/2027	-	-	51,750.00	51,750.00
11/01/2027	300,000.00	5.750%	51,750.00	351,750.00
05/01/2028	-	-	43,125.00	43,125.00
11/01/2028	315,000.00	5.750%	43,125.00	358,125.00
05/01/2029	-	-	34,068.75	34,068.75
11/01/2029	330,000.00	5.750%	34,068.75	364,068.75
05/01/2030	-	-	24,581.25	24,581.25
11/01/2030	355,000.00	5.750%	24,581.25	379,581.25
05/01/2031	-	-	14,375.00	14,375.00
11/01/2031	370,000.00	5.750%	14,375.00	384,375.00
05/01/2032	-	-	3,737.50	3,737.50
11/01/2032	130,000.00	5.750%	3,737.50	133,737.50
<b>Total</b>	<b>\$ 3,270,000.00</b>		<b>\$ 1,177,100.00</b>	<b>\$ 4,447,100.00</b>

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
RECONCILIATION OF STORMWATER COMPLIANCE MONIES  
AUGUST 31, 2022**

<b>Beginning balance</b>		\$ 218,317.74
Kossen		
Invoice #54115	(8,040.75)	
Invoice #55223	(8,040.75)	
Invoice #55961	(8,040.75)	
Invoice #55964	<u>(3,233.00)</u>	(27,355.25)
Panhandle Engineering		
Invoice #60503-1/19	(1,450.00)	
Invoice #60503-1/20	(2,900.00)	
Invoice #60521/01	(1,800.00)	
Invoice #60521/02	(500)	
Invoice #60521/03	(1,000)	
Invoice #60521/04	(500)	
Invoice #60521/07	(3,500)	
Invoice #60521/08	(8,835)	
30-Apr-22	(39,289)	
Invoice #60521/11	<u>(2,000)</u>	(61,774.00)
The Service House		
Invoice #60396	(291.69)	
Invoice #60397	<u>(291.69)</u>	(583.38)
Shark's Tooth Golf Club		
Invoice #60947	(3,180.00)	
Credit memo #63609	<u>908.46</u>	(2,271.54)
Interest income	848.51	
Bank charges	<u>(118.34)</u>	730.17
Remaining available monies		<u><u>\$ 127,063.74</u></u>



**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
REVENUE CERTIFICATES RECEIVABLE**

Payment Date	Property Location	Water	Sewer	Total	Balance Due	WO #
					232,615.41	
3/23/2006	TURTLE COVE CIR, 209	215.69	826.32	1,042.01		7951
	SALAMANDER TRL, 1401-1407	813.20	2,915.47	3,728.67		7984
	SALAMANDER TRL, 1409-1415	813.20	2,915.47	3,728.67		7985
	SALAMANDER TRL, 1417-1423	813.20	2,915.47	3,728.67		7986
	SALAMANDER TRL, 1400-1406	813.20	2,915.47	3,728.67		8962
	SALAMANDER TRL, 1408-1414	813.20	2,915.47	3,728.67		8963
	SALAMANDER TRL, 1416-1422	813.20	2,915.47	3,728.67		8964
	MATCH PT LN, 1506-10 12-16 13-15	1,900.30	7,090.55	8,990.85	200,210.54	8968
5/15/2006	WILD HERON WAY, 823	217.00		217.00		8072
	WILD HERON WAY, 1436	68.20	311.36	379.56	199,613.98	8084
8/3/2006	TURTLE COVE, 300	647.06	894.25	1,541.31	198,072.67	8097
9/13/2006	BAYFLOWER , 1619 LOT B1-10	313.10	1,146.38	1,459.48	196,613.19	8276
7/30/2007	MARSH RABBIT RUN, 22105	294.50	976.54	1,271.04		8431
	GREEN TURTLE LN, 3306	464.48	962.39	1,426.87		8535
30-Apr-22	SALAMANDER TR, 1518 1532	616.90	2,137.07	2,753.97		8570
	SALAMANDER TR, 1562 60 64 70	778.10	2,589.96	3,368.06	187,793.25	8595
1/9/2012	SALAMANDER TR, 1516	226.30	835.02	1,061.32		10138
	SALAMANDER TR, 1530	179.80	622.72	802.52		10139
	SALAMANDER TR, 1534	179.80	622.72	802.52	185,126.89	10140
2/17/2012	LOST COVE, 1721	179.80	622.72	802.52		10249
6/29/2012	SHARKSTOOTH TRL, LOT AV-9	226.30	835.02	1,061.32	183,263.05	10364
7/16/2012	SALAMANDER TRL, 1522	226.30	835.02	1,061.32		10374
	SALAMANDER TRL, 1546	226.30	835.02	1,061.32		10375
	SALAMANDER TRL, 1566	226.30	835.02	1,061.32		10376
7/23/2012	PROSPECT PROMENADE, 1206	226.30	835.02	1,061.32	179,017.79	10381
12/18/2012	LITTLE HAWK DR, 1515	182.90	636.88	819.78		10509
12/20/2012	LOST COVE LN, 1608	260.40	905.78	1,166.18		10511
1/28/2013	SALAMANDER TRL, 1335	170.50	580.27	750.77		10536
2/4/2013	SALAMANDER TRL, 1501	143.79	498.06	641.85		10541
2/4/2013	SKIMMER CT, 1702	176.02	645.21	821.23		10542
2/4/2013	SKIMMER CT, 1710	143.79	498.06	641.85		10544
2/11/2013	SKIMMER CT, 1715	195.30	693.49	888.79		10551
2/11/2013	SALAMANDER TRL, 1116	170.50	580.27	750.77		10552
2/11/2013	SALAMANDER TRL, 1503	170.50	580.27	750.77		10553
2/25/2013	SKIMMER CT, 1709	210.80	764.25	975.05	171,785.83	10559
3/20/2013	SKIMMER CT, 1711	195.30	693.49	888.79		10598
4/22/2013	SKIMMER CT, 1713	155.00	509.50	664.50		10644
5/15/2013	WILD HERON, LOT A1-34	347.20	1,245.45	1,592.65	167,664.85	10669
7/23/2013	SKIMMER CT, 1704	155.00	509.50	664.50	167,000.35	10717
4/4/2014	SHARKSTOOTH TRL, 1602	300.70	1,004.85	1,305.55		10874
	MEADOWLARK WAY, 1603	260.40	905.78	1,166.18		10875
	SKIMMER CT. 1071	186.00	651.03	837.03		10879
	DUNE LAKE TRL, 1523	210.80	764.25	975.05		10969
	SKIMMER CT, 1706	195.30	693.49	888.79		11071
	SMIMMER CT, 1703	155.00	509.50	664.50		11072
	TURTLEBACK TRL, 1416	155.00	509.50	664.50		11073

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
REVENUE CERTIFICATES RECEIVABLE**

<b>Payment Date</b>	<b>Property Location</b>	<b>Water</b>	<b>Sewer</b>	<b>Total</b>	<b>Balance Due</b>	<b>WO #</b>
	LITTLE HAWK DR, 1505	210.80	764.25	975.05		11080
	SKIMMER CT, 1707	195.30	693.49	888.79	158,634.93	11082
6/3/2014	LITTLE HAW LN, 1511	179.80	622.72	802.52		11095
	SHARKS TOOTH TRL, 1601	195.30	693.49	888.79		11134
	SKIMMER CT, 1705	155.00	509.50	664.50	156,279.12	11135
8/5/2014	LOST COVE LN, 1723	195.30	693.49	888.79		11189
	LOST COVE LN, 1732	204.60	735.95	940.55	154,449.79	11223
10/24/2014	LITTLE HAWK LN, 1501	204.60	735.95	940.55		11259
	EGRET LN, 1408	204.60	735.95	940.55		11260
	MEADOWLARK WAY, 1618	266.60	905.78	1,172.38		11277
	LITTLE HAWK LBM 1507	195.31	693.49	888.80		11304
	TURTLE BACK TRL, 22703	285.20	1,019.00	1,304.20		11315
	W WATEROAK BEND, 1214	241.80	905.78	1,147.58		11362
	SALAMANDER TRL, 1520	229.40	849.17	1,078.57	146,977.18	11370
2/12/2015	LITTLE HAWK LN, 1512	210.80	764.25	975.05		11422
	EGRET CT, 1406	210.80	764.25	975.05	145,027.08	11423
6/15/2015	FOX GLEN TRACE, 22207	241.80	905.78	1,147.58		11584
	LITTLE HAWK LN, 1503	235.60	877.47	1,113.07		11587
	LITTLE HAWK LN, 1509	285.20	1,103.92	1,389.12		11622
	SALAMANDER TRL, 1301	155.00	509.50	664.50		11632
	SKIMMER CT, 1708	176.70	608.57	785.27		11666
	SHARKS TOOTH TRL, 1607	193.38	724.45	917.82	139,009.73	11696
03/23/16	CITY OF PANAMA CITY	0.00	0.00	865.60	138,144.13	11698
05/30/16	CITY OF PANAMA CITY	0.00	0.00	743.49	137,400.64	
06/24/16	CITY OF PANAMA CITY	0.00	0.00	52,013.10	85,387.54	
07/28/16				1,438.72	83,948.82	
10/28/16				448.66	83,500.16	
02/28/17	CITY OF PANAMA CITY	0.00	0.00	1,643.59	81,856.57	
08/17/17	CITY OF PANAMA CITY	0.00	0.00	2,779.80	79,076.77	
01/24/18	CITY OF PANAMA CITY	0.00	0.00	3,818.94	75,257.83	
03/07/18	CITY OF PANAMA CITY	0.00	0.00	519.35	74,738.48	
05/30/18	CITY OF PANAMA CITY	0.00	0.00	2,473.61	72,264.87	
07/26/18	CITY OF PANAMA CITY	0.00	0.00	876.64	71,388.23	
03/04/19	WILD HERON WAY, 721	252.65	785.48	1,038.13	70,350.10	
	WILD HERON WAY, 717	249.55	884.55	1,134.10	69,216.00	
	WILD HERON WAY, 709	249.55	884.55	1,134.10	68,081.90	
	LOST COVE LN, 1616	98.95		98.95	67,982.95	
	TURTLE COVE CIR, 313	143.79	430.14	573.93	67,409.02	
	TURTLE COVE CIR, 304	39.46	352.87	392.32	67,016.69	
	WILD HERON WAY, 724	172.61	1,180.55	1,353.16	65,663.53	
	WILD HERON WAY, 728	172.61	1,180.55	1,353.16	64,310.37	
	WILD HERON WAY, 732	172.61	1,180.55	1,353.16	62,957.21	
05/15/19	LOST COVE LN, 1604	33.07		33.07	62,924.13	
	LOST COVE LN, 1612	174.06	1,037.85	1,211.91	61,712.22	
08/29/19	CITY OF PANAMA CITY	0.00	0.00	3,433.48	58,278.74	
04/23/20	CITY OF PANAMA CITY	0.00	0.00	6,445.02	51,833.72	
08/07/20	CITY OF PANAMA CITY	0.00	0.00	5,858.60	45,975.12	

**LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT  
REVENUE CERTIFICATES RECEIVABLE**

<b>Payment Date</b>	<b>Property Location</b>	<b>Water</b>	<b>Sewer</b>	<b>Total</b>	<b>Balance Due</b>	<b>WO #</b>
11/19/20	CITY OF PANAMA CITY	0.00	0.00	66.17	45,908.95	
02/10/21	CITY OF PANAMA CITY	0.00	0.00	3,941.78	41,967.08	
04/05/21	CITY OF PANAMA CITY	0.00	0.00	5,709.60	36,257.48	
08/01/21	CITY OF PANAMA CITY	0.00	0.00	8,293.75	27,963.73	
10/17/21	CITY OF PANAMA CITY	0.00	0.00	10,411.99	17,551.74	
02/18/22	CITY OF PANAMA CITY	0.00	0.00	12,574.96	4,976.78	
07/07/22	CITY OF PANAMA CITY			4,976.78	0.00	

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**MINUTES**

**DRAFT**

**MINUTES OF MEETING  
LAKE POWELL RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

The Board of Supervisors of the Lake Powell Residential Golf Community Development District held a Public Hearing and Regular Meeting on August 1, 2022, at 3:00 P.M., Central Time, at the Boat House, 1110 Prospect Promenade, Panama City Beach, Florida 32413.

**Present and constituting a quorum were:**

David Dean	Chair
Thomas Balduf	Vice Chair
David Holt	Assistant Secretary
Jerry Robinson	Assistant Secretary
Frank Self	Assistant Secretary

**Also present were:**

Cindy Cerbone	District Manager
Mike Burke	District Counsel
Robert Carroll	District Engineer
Bethany Womack	Ecologist/District Operations Manager
Tom Kerins	Resident
Residents	

**FIRST ORDER OF BUSINESS**

**Call to Order/Roll Call**

Ms. Cerbone called the meeting to order at 3:02 p.m., Central Time. All Supervisors were present, in person.

**SECOND ORDER OF BUSINESS**

**Public Comment**

There were no public comments.

**THIRD ORDER OF BUSINESS**

**Public Hearing on Adoption of Fiscal Year  
2022/2023 Budget**

**A. Affidavit of Publication**

40 The affidavit of publication was included for informational purposes.

41 **B. Consideration of Resolution 2022-06, Relating to the Annual Appropriations and**  
42 **Adopting the Budget for the Fiscal Year Beginning October 1, 2022, and Ending**  
43 **September 30, 2023; Authorizing Budget Amendments; and Providing an Effective**  
44 **Date**

45 Ms. Cerbone presented the proposed Fiscal Year 2023 budget, highlighting line item  
46 increases, decreases and adjustments. The funds for the “Road resurfacing” budget line item  
47 was transferred to the “Road projects” fund balance, due to the decision to defer the project.

48 Mr. Dean stated that the project was tabled due to the lack of contractor responses to  
49 the CDD’s Request for Proposals (RFP). Upon project completion, the County will accept the  
50 turnover of Wild Heron Way (WHW), from Highway 98 to the front gate.

51 Ms. Cerbone stated that, going forward, hopefully the CDD and the POA will coordinate  
52 similar projects and hopefully engage one contractor that is able to bill each entity separately.

53

54 **On MOTION by Mr. Holt and seconded by Mr. Dean, with all in favor, the Public**  
55 **Hearing was opened.**

56

57

58 Resident Tom Kerins asked what is driving the budget increase and why the funding for  
59 certain projects, like roadways, has not yet been used. Mr. Dean recalled explaining in prior  
60 meetings that it is due to inflation and allocating significant funds in the event of a natural  
61 disaster that might also necessitate imposing special assessments to cover the costs. Mr. Holt  
62 stated that, due to the difficulty engaging contractors, the CDD asked St. Joe’s if it can  
63 piggyback off their project.

64

65 **On MOTION by Mr. Holt and seconded by Mr. Self, with all in favor, the Public**  
66 **Hearing was closed.**

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68

69 Ms. Cerbone presented Resolution 2022-06.

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**On MOTION by Mr. Self and seconded by Mr. Dean, with all in favor, Resolution 2022-06, Relating to the Annual Appropriations and Adopting the Budget for the Fiscal Year Beginning October 1, 2022, and Ending September 30, 2023; Authorizing Budget Amendments; and Providing an Effective Date, was adopted.**

**FOURTH ORDER OF BUSINESS**

**Consideration of Resolution 2022-07, Making a Determination of Benefit and Imposing Special Assessments for Fiscal Year 2022/2023; Providing for the Collection and Enforcement of Special Assessments, Including but Not Limited to Penalties and Interest Thereon; Certifying an Assessment Roll; Providing for Amendments to the Assessment Roll; Providing a Severability Clause; and Providing an Effective Date**

Ms. Cerbone presented Resolution 2022-07.

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**On MOTION by Mr. Balduf and seconded by Mr. Holt, with all in favor, Resolution 2022-07, Making a Determination of Benefit and Imposing Special Assessments for Fiscal Year 2022/2023; Providing for the Collection and Enforcement of Special Assessments, Including but Not Limited to Penalties and Interest Thereon; Certifying an Assessment Roll; Providing for Amendments to the Assessment Roll; Providing a Severability Clause; and Providing an Effective Date, was adopted.**

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**FIFTH ORDER OF BUSINESS**

**District Counsel – Discussion/  
Consideration/Update**

**A. Sunshine Law Refresher**

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Mr. Burke distributed and presented the Summary of the Sunshine Law Refresher, and discussed requirement for governmental entities to provide public access to public meetings and public records. He provided examples of Supervisors violating the Sunshine Law. He encouraged all Supervisors to view the Attorney General’s website on this matter and to contact him with questions or concerns.

110 **B. Update: Agreements Sent to POA**111 **I. POA Maintenance Agreement**

- 112 •
- Landscape Standards**

113 **II. POA Agreement for Stormwater Facility Management Services**114 **III. POA Boat House Lease**115 **C. Other**

116 These items were deferred to the next meeting.

117

118 **SIXTH ORDER OF BUSINESS**118 **District Engineer – Discussion/  
119 Consideration Update**

120

121 **A. Ratification of Stormwater Management Needs Analysis**122 Mr. Carroll stated that the Stormwater Management Needs Analysis Report was  
123 submitted by the June 30, 2022 due date. Mr. Burke stated ratification is not necessary124 **B. Review of 2015 Wild Heron Way Operational Speed Analysis**

125 The Report was included for informational purposes.

126 Mr. Dean asked for this to be on the agenda as he and others in the community believe  
127 the speed limit coming off Covington Bridge and the guest turn outside the gate should be  
128 reduced, to prevent a possible collision. In his opinion, the speed limit sign three-quarters of a  
129 mile outside the gate was improperly placed and asked that it be removed. Ms. Cerbone stated  
130 that, due to the age of this Report, these requests might require performing another traffic  
131 study and/or speed analysis.132 Discussion ensued regarding updating the Study to include other locations, the POA  
133 currently installing a sign beyond the exit of Shark's Tooth Golf Club due to close calls and the  
134 Board's decision at a prior meeting not to pursue entering into a traffic enforcement agreement  
135 with the Sheriff. Mr. Carroll was asked to confirm if the 2015 Study is still valid and obtain a  
136 proposal if a new one is needed. Upon further review, it was discovered that Page 5 listed  
137 recommendations to post additional signage in advance of the curves in each direction.

138



139 **On MOTION by Mr. Dean and seconded by Mr. Balduf, with all in favor,**  
140 **authorizing Mr. Carroll to purchase and install appropriate signage following**  
141 **the recommendations in the 2015 Wild Heron Way Operational Speed Analysis**  
142 **and remove the 15-mph speed limit sign identified by Mr. Dean but keep the**  
143 **signpost, was approved.**

144  
145  
146 The Board discussed reconsidering entering into a Traffic Enforcement Agreement,  
147 various types of monitoring devices available to address speeding issues and the POA using  
148 private companies for enforcement.

149 A Board Member suggested asking if, as part of the “Great Southern Slowdown”  
150 movement, the Sheriff Department will monitor CDD roadways a few days a week.

151 A resident asked if installing new speed monitoring systems addresses the issue of  
152 drivers running stop signs. Mr. Robinson replied hopefully, if the equipment is placed  
153 accordingly.

154 Discussion ensued about defining CDD and POA roads and enforcing traffic violations.

155 A resident reported his wife’s recent traffic accident on Wild Heron Way and Highway  
156 98 and suggested the CDD ask St. Joe’s to install a traffic light.

157 **The meeting recessed at 4:30 p.m., and reconvened at 4:40 p.m.**

158 **C. Update: Covington Bridge Inspection Report- Bridge #460167**

159 Mr. Carroll presented the Covington Bridge Inspection Report and four Work Order  
160 Clarifications, which included timelines to complete each recommendation. Proposals will be  
161 obtained for each project, to commence next year. Mr. Burke will ask St. Joe to share the cost  
162 of this expense, as Mr. Holt stated that the Panama City Planning Commission is requiring St.  
163 Joe to initiate a Maintenance Agreement with the CDD, due to the recent PUD.

164 Mr. Dean stated that the specification to which the bridge was built was sent to Mr.  
165 Carroll. Mr. Holt opined that the amount the CDD contributes to the County far exceeds the  
166 benefit it receives in resources and, in his opinion, the CDD should continue pushing to get the  
167 bridge transferred to the County. Due to the additional supporting documentation, Ms.  
168 Womack suggested the CDD address this with the County once the bridge repairs are  
169 completed.

170 **D. Update: Drainage Improvements on Intersection of Sweetwater Bay Trail and Wild**  
171 **Heron Way**

172 Mr. Carroll had reviewed the certified documentation and surveys and stated the  
173 contractor should be able to proceed now that the water levels have receded.

174

175 **SEVENTH ORDER OF BUSINESS**

**District Ecologist – Discussion/  
176 Consideration/Update**

177

178 Ms. Womack reported the following

179 ➤ Drainage Improvement Projects: The Marsh Rabbit Run project was almost complete  
180 and the Turtleback Trail project will be next. The Board agreed for the recommendation to  
181 obtain proposals from this contractor to repair the side slopes on CDD roadways.

182 **A. Update: Firewise**

183 Ms. Womack stated that the POA asked for the CDD to service Sawgrass Court. Cutting  
184 overgrowth and removing the fallen dead tree is scheduled for next week. There are no other  
185 Firewise areas to address and, as such, there will be a break before they start mitigation cutting  
186 before getting ready for the annual monitoring in September.

187 In response to the request for the CDD to address overgrowth at the Wild Heron Way  
188 traffic circle and Marsh Rabbit Run, Ms. Womack stated she will investigate the area to  
189 determine what can be done, as this is a conservation easement area.

190 Discussion ensued regarding dead pine trees.

191 **B. Consideration of Proposals for Conservation Area Sign Replacements**

192 Ms. Womack will obtain proposals for the signs and have the POA Maintenance Team  
193 install them.

194 **C. Update: Pond Aerators**

195 Lake Doctors expects delivery of equipment this week and, once installed, the  
196 electrician will connect the power.

197 **D. Status of Areas Allowed to be Managed**

198 Ms. Womack confirmed that most CDD areas are restricted by conservation easements.

199 This item will be removed from future agendas.

200 Ms. Womack presented the New Leaf proposal to remove dead trees on CDD property.

201

202 **On MOTION by Mr. Dean and seconded by Mr. Robinson, with all in favor, the**  
203 **New Leaf proposal to remove dead pine trees in the Linkside area, in a not-to-**  
204 **exceed amount of \$1,900, was approved.**

205

206

207 Ms. Womack stated that she and Mr. Carroll will reinspect the southeast approach of  
208 the bridge inside the gate to determine what is causing the subsistence.

209

210 **EIGHTH ORDER OF BUSINESS**

**Acceptance of Unaudited Financial  
Statements as of June 30, 2022**

211

212

213 Ms. Cerbone presented the Unaudited Financial Statements as of June 30, 2022.

214

215 **On MOTION by Mr. Balduf and seconded by Mr. Holt, with all in favor,**  
216 **Unaudited Financial Statements as of June 30, 2022, were accepted.**

217

218

219 **NINTH ORDER OF BUSINESS**

**Approval of Minutes**

220

221 Ms. Cerbone presented the following:

222 **A. June 6, 2022 Regular Meeting**

223 The following changes were made:

224 Line 25: Change "Pete" to "Paul"

225 Line 156: Change "Robinson" to "Dean"

226 **B. June 13, 2022 Continued Regular Meeting**

227 **C. June 21, 2022 Continued Regular Meeting**

228

229 **On MOTION by Mr. Balduf and seconded by Mr. Robinson, with all in favor, the**  
230 **June 6, 2022 Regular Meeting Minutes, as amended, and the June 13, 2022**  
231 **Continued Regular Meeting Minutes and the June 21, 2022 Continued Regular**  
232 **Meeting Minutes, as presented, were approved.**

233

234

235 **TENTH ORDER OF BUSINESS** **Staff Reports**

236

237 **A. Ecologist/Operations: *Cypress Environmental of Bay County, LLC***

238 There was nothing further to report.

239 **B. District Counsel: *Burke Blue***

240 There was no report.

241 **C. District Engineer: *McNeil Carroll Engineering, Inc.***

242 There was nothing further to report.

243 **D. District Manager: *Wrathell, Hunt and Associates, LLC***244 **I. Speeding and Traffic Enforcement**

245 This item was discussed during Item 6B.

246 **II. NEXT MEETING DATE: October 3, 2022 at 3:00 P.M. (Central Time)**247 **○ QUORUM CHECK**

248 The next meeting will be held on October 3, 2022 at 3:00 p.m., Central Time.

249

250 **ELEVENTH ORDER OF BUSINESS** **Board Member Comments**

251

252 **• Coordination with POA**253 **A. Road Restriping**254 Ms. Cerbone asked Ms. Womack to reinforce with Diane the CDD's interest in  
255 coordinating projects with the POA, such as roadway restriping, signage and other similar  
256 projects.257 **B. Catch Basin Cleanout**258 Mr. Carroll stated that the POA scheduled this project. Ms. Womack stated she will  
259 inspect the CDD roadways and report her findings at the next meeting

260 There were no Board Member comments.

261

262 **TWELFTH ORDER OF BUSINESS** **Public Comments**

263

264 This item was addressed following the Thirteenth Order of Business.

265

266 **THIRTEENTH ORDER OF BUSINESS** **Action Item Recap**

267

268 Ms. Cerbone will email a recap of the Action Items to the Board for their review and  
269 comments.

270 **Public Comments**

271 **This item, previously the Twelfth Order of Business, was presented out of order.**

272 In response to a resident’s request for an update on the tennis court/land swap matter,  
273 it was noted that the title search is pending.

274

275 **FOURTEENTH ORDER OF BUSINESS** **Adjournment**

276

277 There being nothing further to discuss, the meeting adjourned.

278

279 **On MOTION by Mr. Dean and seconded by Mr. Holt, with all in favor, the**  
280 **meeting adjourned at 5:24 p.m., Central Time.**

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[SIGNATURES APPEAR ON THE FOLLOWING PAGE]

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\_\_\_\_\_  
Secretary/Assistant Secretary

\_\_\_\_\_  
Chair/Vice Chair

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**STAFF  
REPORTS  
DI**

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Lt. Byrd,

Thank you again for your call today. Please send me the information to pass along to the board members regarding the below:

- Steps in obtaining an MOU
- Street signs- height, size, etc. of what is enforced by traffic laws
- Mobile speed bump vendors
- Neighborhood watch sign vendors
- Automated systems-speed detectors vendors

Is there a way that you all can provide the speed detectors like the ones you mentioned are out there now, for the CDD? Is this a courtesy for a few weeks at a time or how does this work exactly?

Thank you!

*Jamie*

**Jamie Sanchez**

**District Manager**

**E-Mail: [sanchezj@whhassociates.com](mailto:sanchezj@whhassociates.com)**

**Wrathell, Hunt and Associates, LLC**

2300 Glades Road #410W

Boca Raton, FL 33431

Toll-free: (877)276-0889

Phone: (561)571-0010 ext. 135

Cell: (561)512-9027

Fax: (561)571-0013

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**Wrathell, Hunt and Associates, LLC**



## Jamie Sanchez

---

**From:** Billy Byrd <billy.byrd@bayso.org>  
**Sent:** Tuesday, September 27, 2022 5:25 PM  
**To:** Jamie Sanchez  
**Subject:** Re: Lake Powell CDD

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Good afternoon! As far as an MOU for traffic enforcement, the HOA would need to make all of the traffic signs in compliance with state statutes, FSS 316.0745 /Uniform signals and devices, before we could approach that agreement. This statute will give you guidance on sign heights and sizes. This would include speed limit signs, stop signs, yield signs, one way signs and any other enforceable signs. FDOT.gov will also give you some specifics.

As far as the installation of speed bumps, there are several vendors on line to purchase from and can even be installed by the HOA. Trafficsafetystore.com is a great company to deal with. Safetysign.com can do the signs we were talking about and make them in accordance with state statutes for the MOU.

Be Safe!  
JIC



**BAY COUNTY SHERIFF'S OFFICE**  
- Field Services Division -  
**Special Operations**

**3421 N. Hwy 77**  
**Panama City, Florida 32405**  
**Office: 850.248.2135**



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**From:** Jamie Sanchez <sanchezj@whhassociates.com>  
**Sent:** Wednesday, September 14, 2022 3:58 PM  
**To:** Billy Byrd <billy.byrd@bayso.org>  
**Cc:** Cindy Cerbone <cerbonec@whhassociates.com>; Andrew Kantarzhi <kantarzhia@whhassociates.com>  
**Subject:** Lake Powell CDD

# The Florida Senate

## 2017 Florida Statutes

<u>Title XXIII</u> MOTOR VEHICLES	<u>Chapter 316</u> STATE UNIFORM TRAFFIC CONTROL  <u>Entire Chapter</u>	<b>SECTION 0745</b> <b>Uniform signals and devices.</b>
--------------------------------------	---	--

### **316.0745 Uniform signals and devices.—**

(1) The Department of Transportation shall adopt a uniform system of traffic control devices for use on the streets and highways of the state. The uniform system shall, insofar as is practicable, conform to the system adopted by the American Association of State Highway Officials and shall be revised from time to time to include changes necessary to conform to a uniform national system or to meet local and state needs. The Department of Transportation may call upon representatives of local authorities to assist in the preparation or revision of the uniform system of traffic control devices.

(2) The Department of Transportation shall compile and publish a manual of uniform traffic control devices which defines the uniform system adopted pursuant to subsection (1), and shall compile and publish minimum specifications for traffic control signals and devices certified by it as conforming with the uniform system.

(a) The department shall make copies of such manual and specifications available to all counties, municipalities, and other public bodies having jurisdiction of streets or highways open to the public in this state.

(b) The manual shall provide for the use of regulatory speed signs in work zone areas. The installation of such signs is exempt from the provisions of s. [335.10](#).

(3) All official traffic control signals or official traffic control devices purchased and installed in this state by any public body or official shall conform with the manual and specifications published by the Department of Transportation pursuant to subsection (2).

(4) It shall be unlawful for any public body or official to purchase, or for anyone to sell, any traffic control signal or device unless it conforms with the manual and specifications published by the Department of Transportation and is certified to be of such conformance prior to sale. Any manufacturer or vendor who sells any traffic control signal, guide, or directional sign or device without such certification shall be ineligible to bid or furnish traffic control devices to any public body or official for such period of time as may be established by the Department of Transportation; however, such period of time shall be for not less than 1 year from the date of notification of such ineligibility.

(5) It is unlawful for any public body to manufacture for installation or placement any traffic control signal, guide, or directional sign or device unless it conforms to the uniform system of traffic control devices published by the Department of Transportation. It is unlawful for any public body to sell any traffic control signal, guide, or directional sign or device it manufactures to any nongovernmental entity or person.

(6) Any system of traffic control devices controlled and operated from a remote location by electronic computers or similar devices must meet all requirements established for the uniform system, and, if such a system affects the movement of traffic on state roads, the design of the system shall be reviewed and approved by the Department of Transportation.

(7) The Department of Transportation may, upon receipt and investigation of reported noncompliance and after hearing pursuant to 14 days' notice, direct the removal of any purported traffic control device that fails to meet the requirements of this section, wherever the device is located and without regard to assigned responsibility under s. [316.1895](#). The public agency erecting or installing the same shall immediately bring it into compliance with the requirements of this section or remove said device or signal upon the direction of the Department of Transportation and may not, for a period of 5 years, install any replacement or new traffic control devices paid for in part or in full with revenues raised by the state unless written prior approval is received from the Department of Transportation. Any additional violation by a public body or official shall be cause for the withholding of state funds for traffic control

purposes until such public body or official demonstrates to the Department of Transportation that it is complying with this section.

(8) The Department of Transportation is authorized to permit traffic control devices not in conformity with the uniform system upon showing of good cause.

**History.**—s. 1, ch. 71-135; s. 1, ch. 72-189; s. 1, ch. 73-310; s. 1, ch. 76-31; s. 1, ch. 77-146; s. 1, ch. 80-178; s. 4, ch. 88-91; s. 3, ch. 88-93; s. 95, ch. 99-248; s. 6, ch. 2010-80; s. 6, ch. 2016-239.

**Note.**—Former s. 316.131.

Disclaimer: The information on this system is unverified. The journals or printed bills of the respective chambers should be consulted for official purposes.

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Address: 64 Outwater Lane, Garfield, NJ 07026  
Email: sales@safetysign.com

SafetySign.com / Traffic Signs / Speed Limit Signs / Speed Limit Signs / Speed Limit 25 MPH Sign

## Speed Limit 25 MPH Sign

*MUTCD R2-1*

11 Reviews



**Item #:** Y1973-B9H

**Sign Reads:** Speed Limit 25

**Compliance:**

- MUTCD
- OSHA 1910.1200(c)
- Shipping Declarations

Tweak Sign

Size:

- 12 × 18"
- 18 × 24"**
- 24 × 30"
- 30 × 36"

Material:

- .080" White Rust-Free Aluminum ⓘ
- .080" Engineering Grade Reflective Aluminum ⓘ
- .080" High Intensity Prismatic Reflective Aluminum ⓘ
- .080" Diamond Grade Reflective Aluminum ⓘ

Laminate: None

Mounting: Two 3/8" holes (one at top, one at bottom)

Packaging: Sold Individually

Qty	1-2	3-4	5-9	10-19	20-39	40+
Each	\$54.15	\$51.40	\$48.85	\$47.40	\$45.90	\$44.55

Qty  **Price: \$54.15**  
Low Price Guarantee ⓘ

**+ Add to Cart**

**Ships Friday ⓘ**

Need a quote for a large order? Call 800-274-5271 or chat online.

A Speed Limit 25 MPH Sign is MUTCD compliant for traffic and pedestrian safety compliance. A Speed Limit 25 MPH Sign is a helpful resource to help protect the drivers and pedestrians on public roadways, private drives, and parking lots.

## Recommended Accessories

## 6' Clearline® Heavy-Duty Rubber Speed Bumps

Rated 4.5/5 ★★★★★ stars - Based on 2 reviews.



## On Sale

You save 18% regardless of quantity

<del>\$158.50</del>	<del>\$143.35</del>	\$130.25
Quantity 1-25	Quantity 26-74	Quantity 75+

**IN STOCK - SHIPS TODAY**

### Quick Facts:

- 6' long x 12" wide x 2 1/4" tall
- Five Mounting Holes
- Mounting Hardware Included
- Weight is approx. 55 lbs, recycled rubber
- No channels - solid from surface to bottom
- Will accept end-caps

### Available Included Hardware:

- 5" Lag & Anchor - Concrete or Asphalt
- 3" Concrete Bolts - Concrete Only
- 14" Galvanized REBAR Spikes - Asphalt, Gravel or Dirt
- 12" Steel Spikes - Asphalt or Gravel
- 13" Rebar Steel Spikes - Asphalt or Gravel
- 18" Galvanized Steel Spikes - Asphalt, Gravel or Dirt

## Description ^

Like the standard rubber speed bump from Clearline safely, the 6' Heavy-Duty slows car speeds to 2 to 5 mph. However, unlike the stand bump that has channels molded in the bottom to reduce weight and protect cable or pipes, the Heavy-Duty Speed Bump is solid rubber for long life after years of abuse.

Five recessed bolt holes in the six foot rubber speed bump make it easy to install and remove. Made from recycled tires.

Features Reflective yellow stripes and cats-eye reflectors to ensure visibility at night. The Heavy-Duty bump weighs approximately 55 lbs and is 6 feet long, 12 inches wide, and the highest point on the speed bump is 2.25 inches tall. Includes mounting hardware. Made in Canada.

Meets Standard **Mounting Hardware Included**

Item #:SBRCLHD6S

## Related Products ^

## 6' Easy Rider® Heavy-Duty Rubber Speed Bump

Rated 4.8/5 ★★★★★ stars - Based on 15 reviews.



## On Sale

You save 17% regardless of quantity

<b>\$160.70</b>	<b>\$160.70</b>	<b>\$160.70</b>
<del>\$192.75</del>	<del>\$175.40</del>	
<b>Quantity</b> 1-25	<b>Quantity</b> 26-74	<b>Quantity</b> 75+

**IN STOCK - SHIPS TODAY**

### Quick Facts:

- 6' long x 12" wide x 2 1/4" tall
- Approx. 55 lbs weight - Heavy Duty Recycled Rubber
- Mounting Hardware Included

### Available Included Hardware:

- 18" Galvanized Steel Spikes - Asphalt, Gravel or Dirt
- 3" Concrete Bolts - Concrete Only
- 14" Galvanized REBAR Spikes - Asphalt, Gravel or Dirt
- 5" Lag & Anchor - Concrete or Asphalt
- 12" Steel Spikes - Asphalt or Gravel

## Description ^

Heavy-Duty Speed Bump - Solid rubber (no channels) for demanding applications - High traffic areas or heavy vehicles like trucks and buses. The Heavy Duty 6-foot rubber speed bump safely slows car speeds to 2 to 5 mph. Helps calm and slow traffic in parking lots and private roads. Recessed bolt holes in the six foot rubber speed bump make it easy to install and remove. Made from 100% recycled tires so there's no internal hardware - an added safety benefit.

Reflective yellow stripes and cats-eye reflectors ensure these speed bumps are highly visible at night. Heavy Duty Speed Bumps are 6 feet long, 12 inches wide, with the tallest portion of the speed bump is 2.25 inches. Includes mounting hardware.

### Mounting Hardware Included

Meets Safety Standards: ITE; FHWA  
Item #:SBRHD6S



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Address: 64 Outwater Lane, Garfield, NJ 07026  
Email: sales@safetysign.com

SafetySign.com / Property / Security / Security Signs / Neighborhood Watch Signs / We Report All Suspicious Activities Immediately

## We Report All Suspicious Activities Immediately Sign



**Item #:** W5478-B9H

**Sign Reads:** Warning Protected By Neighborhood Watch. We Report All Suspicious Activities Immediately

**Sign Design:** Neighborhood Watch Symbol

**Compliance:**

- OSHA 1910.1200(c)
- Shipping Declarations

Other configurations of this product are MUTCD TTC Signs.



**Design Your Own Sign**

**Size:**

12 × 18"

18 × 24"

24 × 30"

**Material:**

- .080" White Rust-Free Aluminum ⓘ
- .080" Engineering Grade Reflective Aluminum ⓘ
- .080" High Intensity Prismatic Reflective Aluminum ⓘ
- .080" Diamond Grade Reflective Aluminum ⓘ

**Laminate:** None

**Mounting:** Two 3/8" holes (one at top, one at bottom)

**Packaging:** Sold Individually

Qty	1-2	3-4	5-9	10-19	20-39	40+
Each	\$54.15	\$51.40	\$48.85	\$47.40	\$45.90	\$44.55

Qty

1

**Price: \$54.15**  
Low Price Guarantee ⓘ

**+ Add to Cart**

**Ships Friday** ⓘ

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A We Report All Suspicious Activities Immediately Sign has effective messages to address security policy and protection concerns. A We Report All Suspicious Activities Immediately Sign is a helpful tool to help protect the health and safety of personnel, and is not a replacement for required protective measures for lessening or removing hazards.

## Recommended Accessories

**LAKE POWELL  
RESIDENTIAL GOLF  
COMMUNITY DEVELOPMENT DISTRICT**

**STAFF  
REPORTS  
DII**

**LAKE POWELL RESIDENTIAL GOLF COMMUNITY DEVELOPMENT DISTRICT**

**BOARD OF SUPERVISORS FISCAL YEAR 2022/2023 MEETING SCHEDULE**

**LOCATION**

*Boat House, 1110 Prospect Promenade, Panama City Beach, Florida 32413*

<b>DATE</b>	<b>POTENTIAL DISCUSSION/FOCUS</b>	<b>TIME</b>
<b>October 3, 2022</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>December 5, 2022</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>February 6, 2023</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>March 6, 2023</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>April 3, 2023</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>May 1, 2023</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>June 5, 2023</b>	<b>Regular Meeting</b>	<b>3:00 PM (Central Time)</b>
<b>August 7, 2023</b>	<b>Public Hearing and Regular Meeting</b>	<b>3:00 PM (Central Time)</b>